



CALL NO. 402

CONTRACT ID. 192117

JEFFERSON COUNTY

FED/STATE PROJECT NUMBER 056GR19P025

DESCRIPTION SHELBYVILLE ROAD (US 60)

WORK TYPE ASPHALT RESURFACING

PRIMARY COMPLETION DATE 10/15/2019

LETTING DATE: March 22,2019

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME March 22,2019. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

NO PLANS ASSOCIATED WITH THIS PROJECT.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

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PART I
SCOPE OF WORK

ADMINISTRATIVE DISTRICT - 05

CONTRACT ID - 192117

056GR19P025

COUNTY - JEFFERSON

PCN - MP05600601901

FD05 056 0060 009-012

SHELBYVILLE ROAD (US 60) (MP 9.722) BEGIN APPROXIMATELY 250 FEET EAST OF KY 1819 EXTENDING EAST TO ENGLISH STATION ROAD (MP 11.674), A DISTANCE OF 01.95 MILES.ASPHALT RESURFACING

GEOGRAPHIC COORDINATES LATITUDE 38:14:47.00 LONGITUDE 85:31:36.00

PCN - MP05600601902

FD05 056 0060 012-014

SHELBYVILLE ROAD (US 60) (MP 12.253) BEGIN 200 FEET WEST OF THE INTERSECTION WITH ENGLISH STATION WAY/BECKLEY WOODS DRIVE EXTENDING EAST TO 200' WEST OF FLOYDS FORK BRIDGE (MP 13.632), A DISTANCE OF 01.37 MILES.ASPHALT RESURFACING

GEOGRAPHIC COORDINATES LATITUDE 38:14:20.00 LONGITUDE 85:29:17.00

COMPLETION DATE(S):

COMPLETED BY 10/15/2019

APPLIES TO ENTIRE CONTRACT

CONTRACT NOTES

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website (www.transportation.ky.gov/contract). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

HARDWOOD REMOVAL RESTRICTIONS

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

ACCESS TO RECORDS

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this contract for the purpose of financial audit or program review. Records and other prequalification information confidentially

disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004.

April 30, 2018

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018

FD05 056 0060 009-012

SURFACING AREAS

The Department estimates the mainline surfacing width to be varied 60 to 92 feet.

The Department estimates the total mainline area to be surfaced to be 98,842 square yards.

The Department estimates the shoulder width to be varied 2 to 15 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 3,559 square yards.

FD05 056 0060 012-014

SURFACING AREAS

The Department estimates the mainline surfacing width to be varied 60 to 74 feet.

The Department estimates the total mainline area to be surfaced to be 57,861 square yards.

The Department estimates the shoulder width to be varied 2 to 13 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 3,236 square yards.

ASPHALT MIXTURE

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

INCIDENTAL SURFACING

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

FUEL AND ASPHALT PAY ADJUSTMENT

The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

OPTION A

Be advised that the Department will accept compaction of asphalt mixtures furnished for driving lanes and ramps, at 1 inch (25mm) or greater, on this project according to OPTION A in accordance with Section 402 and Section 403 of the current Standard Specifications. The Department will require joint cores as described in Section 402.03.02 for surface mixtures only. The Department will accept compaction of all other asphalt mixtures according to OPTION B.

MATERIAL TRANSFER VEHICLE (MTV)

Provide and use a MTV in accordance with Sections 403.02.10 and 403.03.05.

SPECIAL NOTE FOR NON-TRACKING TACK COAT

1. DESCRIPTION AND USEAGE. This specification covers the requirements and practices for applying a non-tracking tack asphalt coating. Place this material on the existing pavement course, prior to placement of a new asphalt pavement layer. Use when expedited paving is necessary or when asphalt tracking would negatively impact the surrounding area. This material is not suitable for other uses. Ensure material can “break” within 15 minutes under conditions listed in 3.2.

2. MATERIALS, EQUIPMENT, AND PERSONNEL.

2.1 Non-Tracking Tack. Provide material conforming to Subsection 2.1.1.

2.1.1 Provide a tack conforming to the following material requirements:

Property	Specification	Test Procedure
Viscosity, SFS, 77 ° F	20 – 100	AASHTO T 72
Sieve, %	0.3 max.	AASHTO T 59
Asphalt Residue ¹ , %	50 min.	AASHTO T 59
Oil Distillate, %	1.0 max.	AASHTO T 59
Residue Penetration, 77 ° F	20 max.	AASHTO T 49
Original Dynamic Shear (G*/sin δ), 82 ° C	1.0 min.	AASHTO T 315
Softening Point, ° F	149 min.	AASHTO T 53
Solubility, %	97.5 min.	AASHTO T 44

¹ Bring sample to 212 °F over a 10-15 minute period. Maintain 212 °F for 15-20 minutes or until 30-40 mL of water has distilled. Continue distillation as specified in T59.

2.2. Equipment. Provide a distributor truck capable of heating, circulating, and spraying the tack between 170 °F and 180 °F. Do not exceed 180 °F. Circulate the material while heating. As required by the manufacturer, ensure the spray bar is equipped with #1 or #2 ¼” V-slot Etnyre nozzles. Other nozzles are not acceptable. Arrange the nozzles in the following patterns from left to right:

Nozzle number(s)	Activity	Orientation
1	On	Vertical
2	Off	-
3	On	Horizontal
4 & 5	Off	-
6	On	Horizontal
Continue 2 off and 1 on pattern through rest of spray bar system.		

Ensure the bar can be raised to between 14 and 18” from the roadway.

2.3 Personnel. Ensure the tack supplier has provided training to the contractor on the installation procedures for this product. Make a technical representative from the supplier available at the request of the Engineer.

3. CONSTRUCTION.

3.1 Surface Preparation. Prior to the application of the non-tracking tack, ensure the pavement surface is thoroughly dry and free from dust or any other debris that would inhibit adhesion. Clean the surface by scraping, sweeping, and the use of compressed air. Ensure this preparation process occurs shortly before application to prevent the return of debris pavement. If rain is expected within one hour after application, do not apply material. Apply material only when the surface is dry, and no precipitation is expected.

3.2 Non-tracking Tack Application. Ensure the roadway temperature is a minimum of 40 °F and rising during the application of the tack. This material is not suitable for use in colder temperatures. Prior to applying the tack, demonstrate competence in applying the tack according to this note to the satisfaction of the Engineer. Heat the tack in the distributor to between 170 – 180 °F. After initial heating to between 170 – 180 °F, the material may be sprayed between 165 °F and 180 °F. Do not apply outside this temperature range. Apply material at a rate of 0.50 pounds (0.06 gallons) per square yard. Ensure full coverage of the material on the pavement surface. Full coverage of this material is critical. If full coverage is not achieved, material application rate may be increased to ensure full coverage. Do not heat material more than twice in one day.

3.3 Non-tracking Tack Certification. Furnish the tacks certification to the Engineer stating the material conforms to all requirements herein prior to use.

3.4 Sampling and Testing. The Department will require a sample of non-tracking tack be taken from the distributor at a rate of one sample per 15,000 tons of mix. Take two 1 gallon samples of the heated material and forward the sample to the Division of Materials for testing within 7 days. Ensure the product temperature is between 170 and 180 °F at the time of sampling.

4. MEASUREMENT. The Department will measure the quantity of non-tracking tack in tons. The Department will not measure for payment any extra materials, labor, methods, equipment, or construction techniques used to satisfy the requirements of this note. The Department will not measure for payment any trial applications of non-tracking tack, the cleaning of the pavement surface, or furnishing and placing the adhesive. The Department will consider all such items incidental to the non-tracking tack.

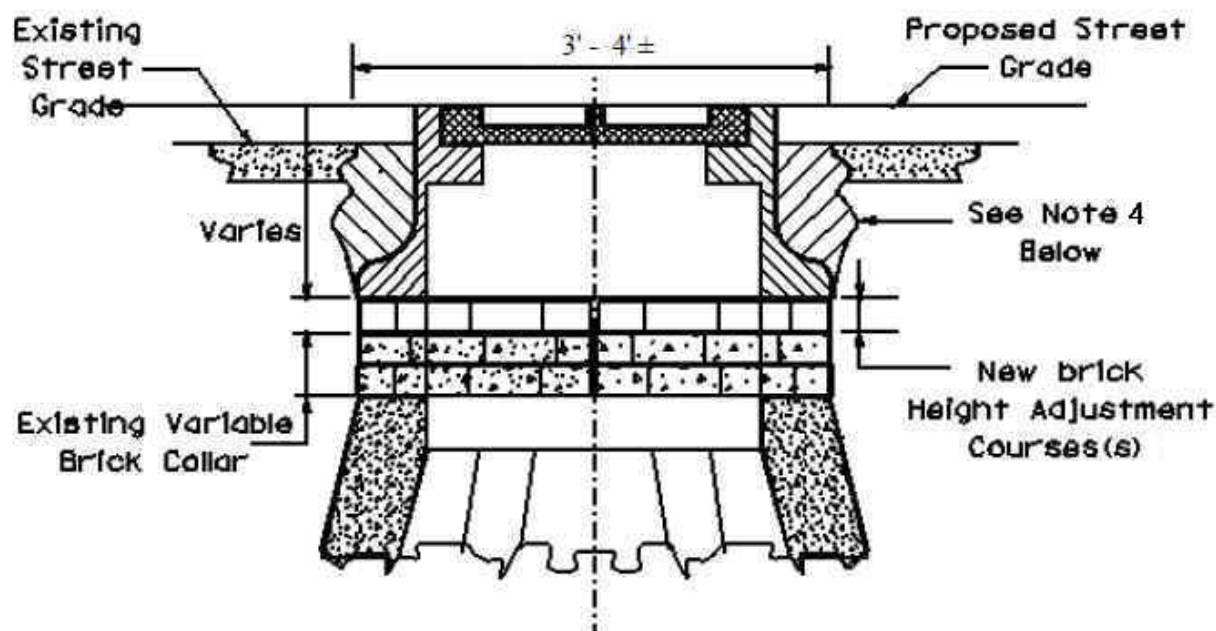
5. PAYMENT. The Department will pay for the non-tracking tack at the Contract unit bid price and apply an adjustment for each manufacturer's lot of material based on the degree of compliance as defined in the following schedule. When a sample fails on two or more tests, the Department may add the deductions, but the total deduction will not exceed 100 percent.

Non-Tracking Tack Price Adjustment Schedule						
Test	Specification	100% Pay	90% Pay	80% Pay	50% Pay	0% Pay
Viscosity, SFS, 77 ° F	20 – 100	19 - 102	17 - 18	15 - 16	14	≤13
			103 - 105	106 - 107	108 - 109	≥ 110
Sieve, %	0.30 max.	≤ 0.40	0.41 - 0.50	0.51 - 0.60	0.61 - 0.70	≥ 0.71
Asphalt Residue, %	50 min.	≥49.0	48.5 – 48.9	48.0 – 48.4	47.5-47.9	≤ 47.4
Oil Distillate, %	1.0 max.	≤1.0	1.1-1.5	1.6 - 1.7	1.8-1.9	>2.0
Residue Penetration, 77 ° F	20 max.	≤ 21	22 - 23	24 - 25	26 - 27	≥ 28
Original Dynamic Shear (G*/sin δ), 82 ° C	1.0 min.	≥0.95	0.92 – 0.94	0.90 – 0.91	0.85 - 0.89	≤ 0.84
Softening Point, ° F	149 min.	≥145	142 - 144	140 - 141	138 - 139	≤ 137
Solubility, %	97.5 min.	≥ 97.0	96.8 – 96.9	96.6 – 96.7	96.4 – 96.5	≤ 96.3

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
24970EC	Asphalt Material for Tack Non-Tracking	Ton

April 30, 2018

SPECIAL NOTE FOR MANHOLE ADJUSTMENT LOUISVILLE METROPOLITAN SEWER DISTRICT (MSD)



NOT TO SCALE

CONSTRUCTION NOTES:

1. The Engineer will determine whether manhole adjustments are to be made prior to or after milling and/or resurfacing.
2. Reuse existing frames, grates, and covers. Use other materials conforming to MSD's Technical Specifications. Obtain these specifications from MSD's Engineering Division Office, 400 South Sixth Street, Louisville, KY 40202.
3. Report any missing or damaged frame, grate, or cover discovered by the Contractor to MSD's inspector. Obtain replacement hardware items from MSD's Storage Yard, 151 Cabel Street, Louisville, KY, upon presentation of an MSD inspector's validated Stores Requisition and exchange for the damaged hardware items.
4. Adjust manhole frame and grates with brick and mortar as shown on the drawing or as directed by the Engineer. Do not use wood shims or blocks to adjust or reset the frame height. Fill cross-hatched area with concrete having a minimum 28-day compressive strength of 2000 psi. Use first class workmanship in conformity with MSD's Technical Specifications.
5. Adjust catch basin frames and grates in similar manner as manhole adjustments as approved by the Engineer.
6. Louisville MSD and/or the Engineer may inspect manhole and/or catch basin frame height adjustments. Make corrections as directed by the Engineer at no additional cost to the Department.
7. The Department will measure and pay Adjust Manhole Frame to Grade according to Sections 403.04.02 and 403.05.01 or Sections 408.04.04 and 408.05.02 as applicable.

SPECIAL NOTE FOR PAVEMENT WEDGE AND SHOULDER SEPARATE OPERATION

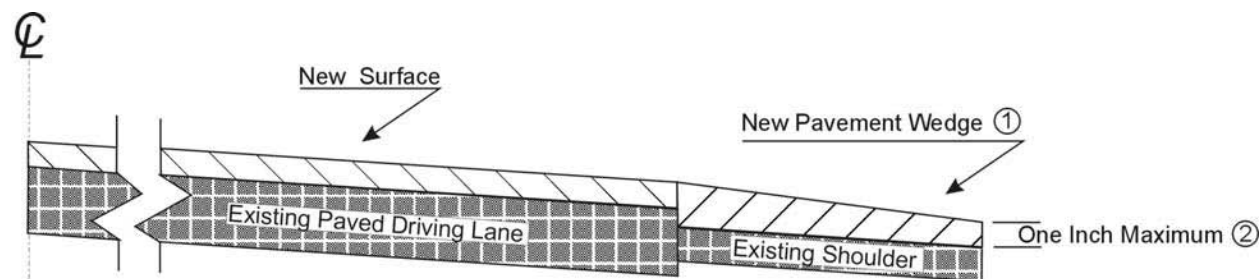
1.0 MATERIALS. Provide an Asphalt Mixture for Pavement Wedge conforming to Section 407 of the Standard Specifications or an Asphalt Surface Mixture conforming to Section 403 of the Standard Specifications, as applicable to the project, for the pavement wedge.

2.0 CONSTRUCTION. Place the Asphalt Mixture for Pavement Wedge or Asphalt Surface Mixture as a separate operation from the driving lane. Prime the existing shoulder with tack material as the Engineer directs before placing the wedge. Construct according to Sections 407.03 and 403.03 as applicable.

When the Engineer deems it appropriate to pave both the driving lane and the adjoining wedge monolithically, equip the paver with a modified screed that extends the full width of the wedge being placed and is tapered to produce a wedge. Obtain the Engineer's approval of the modified screed before placing shoulder wedge monolithically with the driving lane.

The wedge may vary in thickness at the edge of the driving lanes. Where existing site conditions permit, limit the outside edge thickness of the new paving limits to one inch above the existing shoulder wedge elevation. If an Asphalt Surface Mixture is furnished for the pavement wedge, texture according to Section 403.03.08.

The following sketch is primarily for the computation of quantities; however, the wedge will result in a similar cross-section where sufficient width exists. Do not construct a shoulder for placing the wedge unless specified elsewhere in the Contract.



- ① Slope varies, but is down from the driving lanes except on outside of some curves where superelevation controls.
- ② Where existing site conditions permit.

3.0 MEASUREMENT. The Department will measure Asphalt Mixture for Pavement Wedge or Asphalt Surface Mixture placed as the pavement wedge according to Sections 403 and 407 as applicable.

4.0 PAYMENT. The Department will make payment for the completed and accepted quantities of Asphalt Surface Mixtures placed as pavement wedge according to Section 403. The Department will make payment for the completed and accepted quantities of Asphalt Mixture for Pavement Wedge according to Section 407.

SPECIAL NOTE FOR PAVEMENT WEDGE AND SHOULDER MONOLITHIC OPERATION

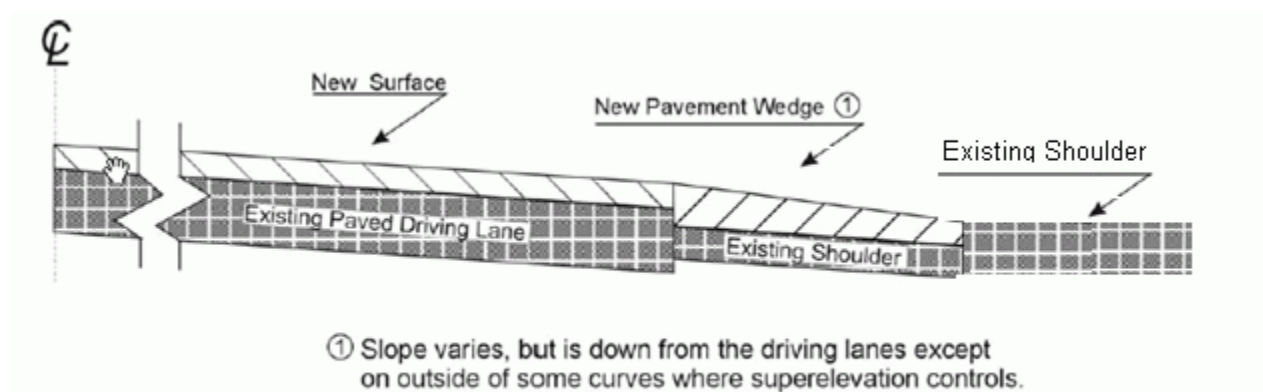
1.0 MATERIALS. Provide an Asphalt Surface Mixture conforming to Section 403 of the Standard Specifications, as applicable to the project, for the pavement wedge.

2.0 CONSTRUCTION. Place the specified Asphalt Surface Mixture on shoulders monolithically with the driving lane. Prime the existing shoulder with tack material as the Engineer directs before placing the wedge. Construct according to Section 403.03 of the Standard Specifications.

Equip the paver with a modified screed that extends the full width of the wedge being placed and is tapered to produce a wedge. Obtain the Engineer's approval of the modified screed before placing shoulder wedge monolithically with the driving lane.

The wedge may vary in thickness at the edge of the milled area in the shoulder. If the area to receive the shoulder wedge is milled prior to placement, during rolling operations pinch the outside edge of the new inlay wedge to match the existing shoulder elevation not being resurfaced. Unless required otherwise by the Contract, construct rolled or sawed rumble strips according to Section 403.03.08, as applicable.

The following sketch is primarily for the computation of quantities; however, the wedge will result in a similar cross-section where sufficient width exists. Do not construct a shoulder for placing the wedge unless specified elsewhere in the Contract.



3.0 MEASUREMENT. The Department will measure Asphalt Surface Mixture placed as the pavement wedge according to Section 403.

4.0 PAYMENT. The Department will make payment for the completed and accepted quantities of Asphalt Surface Mixtures on pavement wedges according to Section 403.

**SPECIAL NOTE FOR
ASPHALT MILLING AND TEXTURING**

Begin paving operations within **48 hours** of commencement of the milling operation. Continue paving operations continuously until completed. If paving operations are not begun within this time period, the Department will assess liquidated damages at the rate prescribed by Section 108.09 until such time as paving operations are begun.

Take possession of the millings and recycle the millings or dispose of the millings off the Right-of-Way at sites obtained by the Contractor at no additional cost to the Department.

1-3520 48 hours Contractor keeps millings
01/2/2012

SPECIAL NOTE FOR BASE FAILURE REPAIR

Repair locations listed on the summary are approximate only. The Engineer will determine actual repair locations and dimensions at the time of construction. Prior to overall milling and/or leveling and wedging, excavate the designated base failure areas to a depth 12 inches below the existing asphalt pavement surface level. Dispose of the excavated materials at waste sites off the Right-of-Way obtained by the Contractor at no additional cost to the Department. See Special Note for Waste and Borrow.

Backfill the excavated areas with Class 3 Asphalt Base 1.00D PG64-22. Compact the asphalt base to the compaction required in Section 403.03.10. Seal the asphalt base with leveling and wedging. Perform all base failure repairs in such a manner that removal and replacement are completed on the same day. Do this work as one of the Contractor's first operations in order to allow further compaction by traffic. Do not mill or place new asphalt surface over repaired base failure areas until a minimum of 7 calendar days have elapsed after placement of the asphalt base. After a minimum of 7 calendar days and when the Engineer determines the base failure repair areas have sufficiently stabilized, begin milling and/or resurfacing operations. Prior to milling and/or constructing the new asphalt surface, level and wedge any settlement of the repair areas.

The bidder must draw conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and will not consider any claim for additional compensation if the materials encountered that are not in accord with the classification shown.

Accept payment at the Contract unit prices per square yard for Base Failure repair and per ton for Leveling and Wedging as full compensation for all labor, materials, equipment, and incidentals for removing pavement and disposing of the materials, furnishing and placing asphalt base, leveling and wedging, and all other items necessary to complete the work according to these notes to the satisfaction of the Engineer.

1-3606basefailurerepairmillinlaypaybysy
01/02/2012

SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS

Consider the dimensions shown on the typical sections for pavement and shoulder widths and thickness' to be nominal or typical dimensions. The Engineer may direct or approve varying the actual dimensions to be constructed to fit existing conditions. Do not widen existing pavement or shoulders unless specified elsewhere in this proposal or directed by the engineer.

1-3725 Typical Section Dimensions
01/02/2012

TRAFFIC CONTROL PLAN

TRAFFIC CONTROL GENERAL

Except as provided herein, maintain and control traffic in accordance with the Standard and Supplemental Specifications and the Standard and Sepia Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic".

Contrary to Section 106.01, furnish new, or used in like new condition, traffic control devices at the beginning of the work and maintain in like new condition until completion of the work.

PROJECT PHASING & CONSTRUCTION PROCEDURES

WORKING HOURS ARE AS FOLLOWS: SUNDAY THROUGH SATURDAY 8:00 P.M. TO 6:00 A.M.

The Engineer may specify additional days and hours when lane closures will not be allowed.

At locations with three or more lanes, maintain one lane of traffic in each direction at all times during construction. At locations with two lanes, maintain alternating one way traffic during construction. Provide a minimum clear lane width of 11 feet; however, provide for passage of vehicles of up to 16 feet in width. If traffic should be stopped due to construction operations, and a school bus on an official run arrives on the scene, make provisions for the passage of the bus as quickly as possible.

LANE CLOSURES

Do not leave lane closures in place during non-working hours.

SIGNS

Sign posts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, only long-term signs (signs intended to be continuously in place for more than 3 days) will be measured for payment. Short-term signs (signs intended to be left in place for 3 days or less) will not be measured for payment but will be incidental to Maintain and Control Traffic.

Traffic Control Plan
Page 2 of 4

CHANGEABLE MESSAGE SIGNS

If deemed necessary by the Engineer, the Department will furnish, operate, and maintain Changeable Message Signs.

ARROW PANELS

Use arrow panels as shown on the Standard Drawings or as directed by the Engineer. The Department will measure for payment the maximum number of arrow panels in concurrent use at the same time on a single day on all sections of the contract. The Department will measure for payment the maximum number of Arrow Panels in concurrent use at the same time on a single day on all sections of the contract. The Department will measure individual Arrow Panels only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. The Department will not measure replacements for damaged Arrow Panels or for panels signs the Engineer directs be replaced due to poor condition or readability for payment. Retain possession of the Arrow Panels upon completion of the work.

TEMPORARY ENTRANCES

The Engineer will not require the Contractor to provide continuous access to farms, single family, duplex, or triplex residential properties during working hours; however, provide reasonable egress and ingress to each such property when actual operations are not in progress at that location. Limit the time during which a farm or residential entrance is blocked to the minimum length of time required for actual operations, not extended for the Contractor's convenience, and in no case exceeding six (6) hours. Notify all residents twenty-four hours in advance of any driveway or entrance closings and make any accommodations necessary to meet the access needs of disabled residents.

Traffic Control Plan
Page 3 of 4

Except as allowed by the Phasing as specified above, maintain direct access to all side streets and roads, schools, churches, commercial properties and apartments or apartment complexes of four or more units at all times.

The Department will measure asphalt materials required to construct and maintain any temporary entrances which may be necessary to provide temporary access; however, the Department will not measure aggregates, excavation, and/or embankment, but shall be incidental to Maintain and Control Traffic. The Engineer will determine the type of surfacing material, asphalt or aggregate, to be used at each entrance.

TRAFFIC SIGNAL LOOPS

Install traffic signal loops according to the Special Notes for Traffic Signal Loop Replacement. Coordinate the placement of the loops with the Engineer.

THERMOPLASTIC INTERSECTION MARKINGS

Consider the locations listed on the summary as approximate only. Prior to milling and/or resurfacing, locate and document the locations of the existing markings. After resurfacing, replace the markings at their approximate existing locations or as directed by Engineer. Place markings not existing prior to resurfacing as directed by the Engineer.

BARRICADES

The Department will not measure barricades used in lieu of barrels and cones for channelization or delineation, but shall be incidental to Maintain and Control Traffic according to Section 112.04.01.

The Department will measure barricades used to protect pavement removal areas in individual units. Each. The Department will measure for payment the maximum number of barricades in concurrent use at the same time on a single day on all sections of the contract. The Department will measure individual barricades only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. The Department will not measure replacements for damaged barricades the Engineer directs to be replaced due to poor condition or reflectivity. Retain possession of the Barricades upon completion of the work.

PAVEMENT MARKINGS

If there is to be a deviation from the existing striping plan, the Engineer will furnish the Contractor a striping plan prior to placement of the final surface course. Install Temporary Striping according to Section 112 with the following exceptions:

Traffic Control Plan
Page 4 of 4

1. Include edge lines in Temporary Striping; and
2. Place Temporary or Permanent Striping before opening a lane to traffic; and
3. If the Contractor's operations or phasing requires temporary markings that must subsequently be removed from the final surface course, use an approved removable lane tape; however, the Department will not measure removable lane tape for separate payment, but will measure and pay for removable lane tape as temporary striping.

PAVEMENT EDGE DROP-OFFS

Do not allow a pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation with an elevation difference greater than 1½". Place Warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual post the signs on both sides of the traveled way. Wedge all transverse transitions between resurfaced and unresurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course.

Protect pavement edges that traffic is not expected to cross, except accidentally, as follows:

Less than 2" - No protection required.

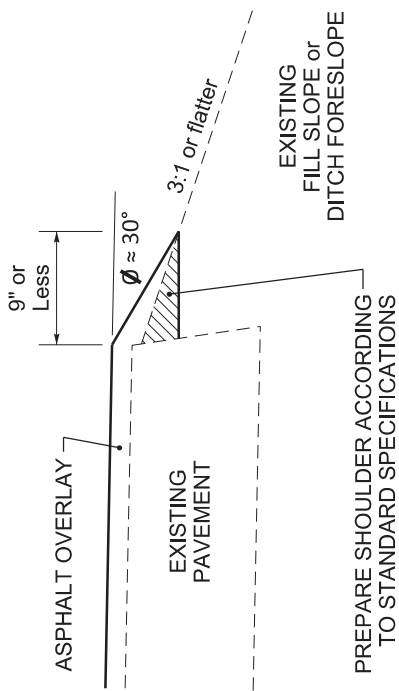
2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. During daylight working hours only, the Engineer will allow the Contractor to use cones in lieu of plastic drums, panels, and barricades. Wedge the drop-off with DGA or asphalt mixture for leveling and wedging with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

Greater than 4" - Protect drop-offs greater than 4 inches within 10 feet of traffic by placing drums, vertical panels, or barricades every 25 feet. The Engineer will not allow the use of cones in lieu of drums, vertical panels, or barricades for drop-offs greater than 4". Place Type III Barricades directly in front of the drop-off facing on coming traffic in both directions of travel. Provide warning signs as shown on the Standard Drawings or as directed by the Engineer

Pedestrians & Bicycles - Protect pedestrian and bicycle traffic as directed by the engineer.

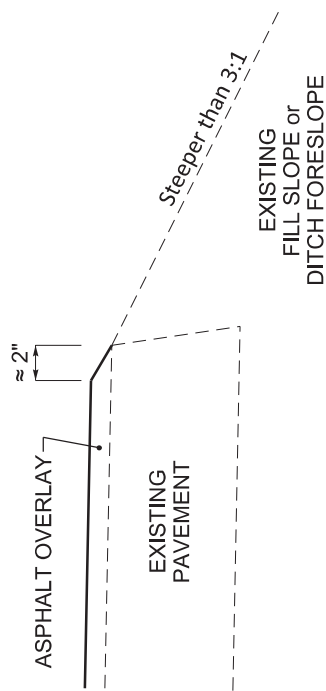
DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to fill slope or ditch foreslope that is 3:1 or less)



DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to fill slope or ditch foreslope that is steeper than 3:1)



NOTES

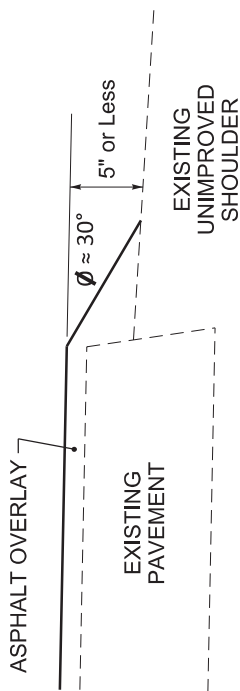
1. DETAILS DO NOT APPLY TO OVERLAYS LESS THAN 1 INCH THICK.
2. THE DURABLE PAVEMENT EDGE DEVICE MAY BE DISENGAGED AT DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT, AS APPROVED BY THE ENGINEER.

DURABLE PAVEMENT EDGE
DETAILS

DRAWING NOT TO SCALE

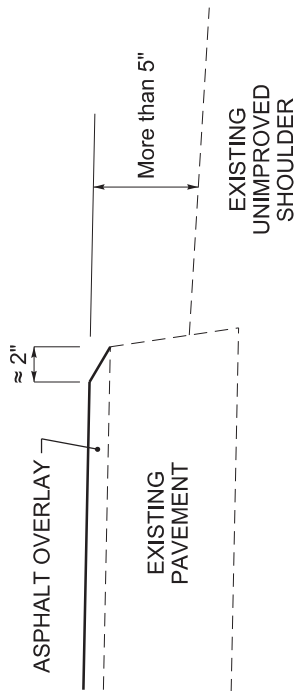
DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to low shoulder with dropoff of 5 inches or less)



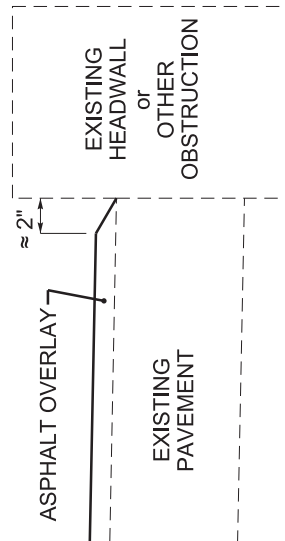
DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to low shoulder with dropoff of more than 5 inches)



DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to an obstruction, such as an existing headwall)



SPECIAL NOTE FOR TRAFFIC SIGNAL LOOP DETECTORS

1.0 DESCRIPTION. Be advised that there are existing traffic signal loop detectors within the construction limits of this project. Except as specified herein, perform traffic signal loop replacement in accordance with the Department's Standard/Supplemental Specifications, Special Provisions, Special Notes, and Standard/Sepia Drawings, current editions and as directed by the Engineer. Article references are to the Standard Specifications. Furnish all materials, labor, equipment, and incidentals for replacement of traffic signal loop installation(s) and all other work specified as part of this contract.

1.1 Pre-bid Requirements. Each Contractor submitting a bid for this work shall make a thorough inspection of the site prior to submitting his bid and shall thoroughly familiarize himself with existing conditions so that the work can be expeditiously performed after a Contract is awarded. Information provided in the Plans regarding types and quantities of work is not to be taken as an accurate or complete evaluation of the materials and conditions to be encountered during construction. The bidder must make his own determinations as to the conditions encountered.

2.0 MATERIALS. Except as specified herein, furnish materials in accordance with Subsection 732.02 and Section 835. Provide for materials to be sampled and tested in accordance with the Department's Sampling Manual. Make materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing, unless otherwise specified in this Special Note.

2.1 Maintain and Control Traffic. See Traffic Control Plan.

2.2 Sand. Furnish natural sand meeting the requirements of Subsection 804.04.01.

2.3 Seeding. Furnish Seed Mix Type I.

2.4 Loop Saw Slot and Fill. Furnish loop sealant, backer rod, and non-shrink grout according to the Saw Slot Detail.

2.5 Junction Boxes. Furnish junction box type B, #57 aggregate, and geotextile filter type IV according to junction box detail.

2.6 Cable No. 14/1 Pair (Lead-in). Furnish cable that is specified in Section 835. Cable shall be ran splice free. This shall include splice kits to connect to the loop wire.

2.7 Conduit. Furnish and install appropriate conduit from transitions to the roadway, junction boxes and poles. See details below.

3.0 CONSTRUCTION. Except as specified herein, install and test Traffic Signal Loop Detectors in accordance with Section 723 and the drawings.

3.1 Testing. The Contractor shall test all loops and cable no. 14/1 pair (lead-in) according to Subsection 723.03.17 before and after milling the roadway. The Contractor may have to separate the loop from the lead-in to perform this test. If the loop/lead-in meets the requirement in Section 723 at the controller cabinet, the loop/lead-in shall not be replaced. If existing loops do not meet the requirements in Subsection 723.03.17, the loops shall be replaced. Replacement loops may be installed either before or after the milling process.

Traffic Signal Loop Detectors Page 2 of 8

The Contractor shall verify that loops (both existing and replacement loops) meet the requirements per Subsection 723.03.17 before the final surface is laid. If loops do not meet conditions of Subsection 723.03.17, the Contractor shall replace them before the resurfacing activities begin. If replacement loops have to be reinstalled, the costs of reinstallation shall be incidental to the milling bid item. The Contractor shall re-splice loops to the lead-in with the proper splice as noted in the spec book.

3.2 Coordination. Notify the Engineer in writing, two (2) weeks prior to beginning any work. The Engineer will contact the District Traffic Engineer to coordinate the Department's operations with the Contractor's work.

3.3 Connection. The Contractor shall schedule all signal loop installation to ensure the new loops are connected to the lead-in and operational within 5 calendar days of the old loops being damaged and/or disconnected. This requirement includes damage caused by any work activity associated with the project. If the new signal loops are not functioning as intended following 5 calendar days, the Department may assess Liquidated Damages at a rate of \$500 per calendar day per signal location until the loops are operating at pre-construction conditions. All liquidated damages will be applied cumulatively.

3.4 Maintain and Control Traffic. See Traffic Control Plan.

3.5 Milling. On projects involving milling and texturing of the existing pavement, install loops in the existing pavement before or after performing the milling and texturing. After milling, the remnant contents of the existing saw slot (grout, loop wires, backer rod, and/or loop sealant) may not be flush with the top of the milled portion of the surface. In such cases, clear the saw slot of loose remnant contents and refill the saw slot with natural sand. Obtain the Engineer's approval of the stabilized saw slot prior to resurfacing. The Department will not measure for separate payment clearing and stabilizing the saw slot and shall consider this work incidental to milling.

3.6 Loop Saw Slot and Fill. The following is a typical step by step procedure for the installation of a loop.

- 1) Carefully mark the slot to be cut, perpendicular to the flow of traffic and centered in the lane.
- 2) Make each saw-cut 3/8-inch wide and at a depth such that the top of the backer rod is a minimum of 4 inches below the surface of asphalt pavement.
- 3) Drill a 1½ inch core hole at each corner and use a chisel to smooth corners to prevent sharp bends in the wire.
- 4) Clean all foreign and loose matter out of the slots and drilled cores and within 1 foot on all sides of the slots using a high pressure washer.
- 5) Completely dry the slots and drilled cores and within 1 foot on all sides of the slots.
- 6) Measure 9-12 inches from the edge of the paved surface (shoulder break or face of curb) and drill a 1½ inch hole on a 45° angle to the conduit adjacent to the roadway.
- 7) Closely inspect all cuts, cores, and slots for jagged edges or protrusions prior to the placement of the wire. All jagged edges and protrusions shall be ground or re-cut and cleaned again.

Traffic Signal Loop Detectors

Page 3 of 8

- 8) Place the loop wire splice-free from the termination point (cabinet or junction box) to the loop, continue around the loop for two turns (6'x30' loop) or three turns (6'x6' loop), and return to the termination point.
- 9) Push the wire into the saw slot with a blunt object such as a wooden stick. Make sure that the loop wire is pushed fully to the bottom of the saw slot. Screwdrivers shall not be used.
- 10) Install duct sealant to a minimum of 1 inch deep into the cored 1½ inch hole.
- 11) Apply loop sealant from the bottom up and fully encapsulate the loop wires in the saw slot. The wire should not be able to move when the sealant has set.
- 12) Cover the encapsulated loop wire with a continuous layer of backer rod along the entire loop and home run saw slots such that no voids are present between the loop sealant and backer rod.
- 13) Finish filling the saw cut with non-shrinkable grout per manufacturer's instructions. Alleviate all air pockets and refill low spaces. There shall be no concave portion to the grout in the saw slot. Any excess grout shall be cleaned from the roadway to alleviate tracking.
- 14) Clean up the site and dispose of all waste off the project.
- 15) Ensure that the grout has completely cured prior to subjecting the loop to traffic. Curing time varies with temperature and humidity.

3.7 Final Dressing, Clean Up, and Seeding. After all work is completed, clean work sites and all disturbed areas. Dispose of all waste and debris off the right of way at sites obtained by the Contractor at no additional cost to the Department. Sow all disturbed earthen areas with Seed Mix Type I.

3.8 Removal. The Contractor shall remove all existing junction boxes, wire from spans/poles/junction boxes/conduits, and conduits. The removal will be incidental to the project.

3.9 Property/Roadway Damage. The Contractor shall be responsible for all damage to public and/or private property resulting from the work. Upon completion of the work, restore all disturbed highway features and private property in like kind design and materials at no additional cost to the Department.

3.10 Right-of-Way Limits. The Department has not established exact limits of Right-of-Way. Limit work activities to obvious Right-of-Way and work areas secured by the Department through Consent and Release of the adjacent property owners. Contractor is responsible for all encroachments onto private lands.

3.11 Utility Clearance. Work around and do not disturb existing utilities. The Department does not anticipate any utility impacts for loop installation. If utilities are impacted, work with associated utility companies to resolve issues.

3.12 Control. Obtain the Engineer's approval of all designs required to be furnished by the Contractor prior to incorporation into the work. The Department reserves the right to permit other contractors, state forces, public utility companies, and others to do work during the construction within the limits of, or adjacent to, the project. Conduct operations and cooperate with such other parties so that interference with each other's work will be reduced to a minimum. The Contractor agrees to make no claims against the Department for additional compensation due to delays or other conditions created by

Traffic Signal Loop Detectors
Page 4 of 8

the operations of such other parties. Should a difference of opinion arise as to the rights of the Contractor and others working within the limits of, or adjacent to, the project, the Engineer will decide as to the respective rights of the various parties involved in order to ensure the completion of the work in general harmony and in a satisfactory manner, and the Engineer's decision shall be final and binding upon the Contractor.

3.13 Bore and Jack. Except for situations outlined in 3.15, bore and jack will be used if conduit is under pavement of any kind. The conduit shall be 2" rigid steel conduit under all pavement areas except for the area where the loop transitions from the saw slot. The installation of conduit should follow the below detail.

3.14 Open Cut Roadway. With permission of the Engineer, roadway may be open cut if the conduit is under pavement. The conduit shall be 2" rigid steel conduit under all pavement areas except for the area that the loop transition from the saw slot. The installation of conduit should follow requirements per Section 723.

4.0 MEASUREMENT. See Subsection 723.04 for bid item notes. Additional bid items include the following:

4.1 Loop Test. The Department will measure the quantity as each individual unit loop tested. The Department will not measure disconnection, reconnection, traffic control, re-splicing per specifications, before and after testing per note above, and any associated hardware for payment and will consider them incidental to this item of work.

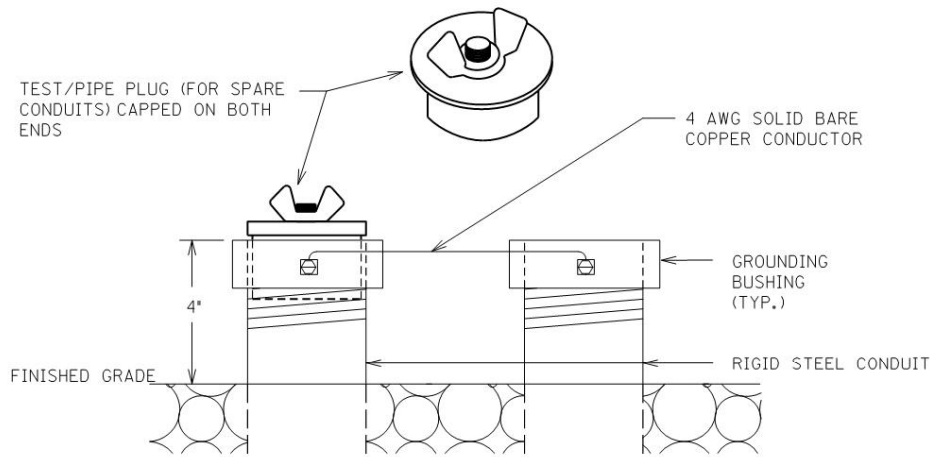
5.0 PAYMENT. The Department will make payment for the completed and accepted quantities of listed items according to Subsection 723.05 in addition to the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
Conduit 1"	4792	Linear Foot
PVC Conduit – 1 ¼ inch – sch 80	24900EC	Linear Foot
PVC Conduit – 2 inch – sch 80	24901EC	Linear Foot
Conduit 2"	4795	Linear Foot
Electrical Junction Box type B	4811	Each
Loop Test	24963ED	Each
Trenching and Backfilling	4820	Linear Foot
Loop Wire	4830	Linear Foot
Cable-No. 14/1 Pair	4850	Linear Foot ¹
Loop Saw Slot and Fill	4895	Linear Foot ¹
Bore and Jack Conduit	21543EN	Linear Foot ³
Open Cut Roadway	4821	Linear Foot ³

The Department will consider payment as full compensation for all work required under these notes and the Standard Specifications.

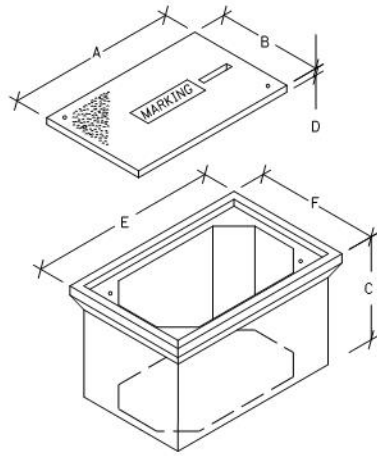
Traffic Signal Loop Detectors

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TEST/PIPE PLUG(FOR SPARE CONDUITS) AND GROUNDING DETAIL

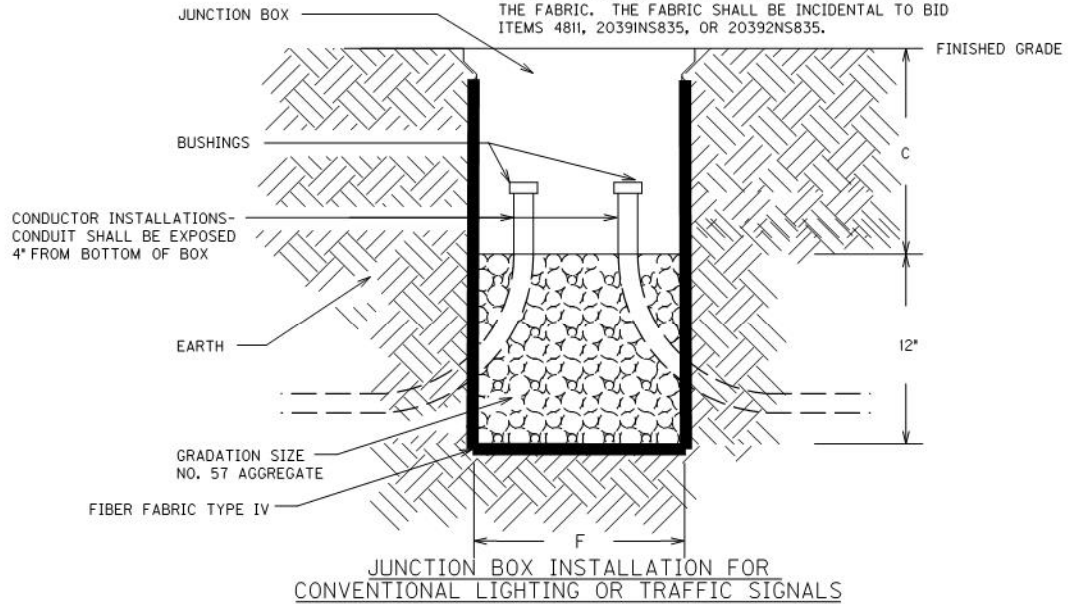
Traffic Signal Loop Detectors
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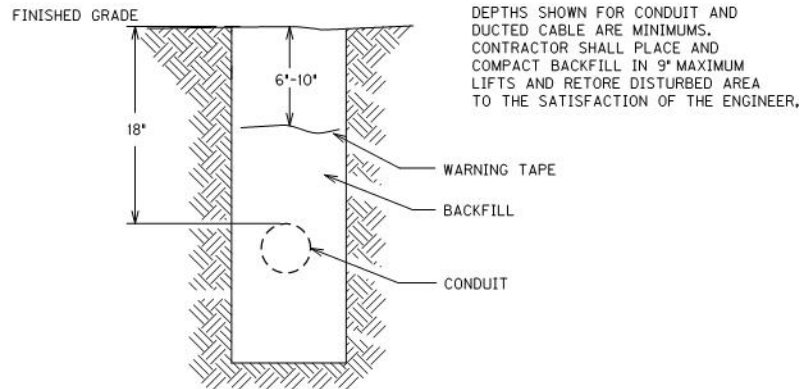
JUNCTION BOX DIMENSIONS (NOMINAL)						
	A	B	C	D	E	F
TYPE A	23"	14"	27"	2"	25"	15"
TYPE B	18"	11"	12"	1 3/4"	20"	13"
TYPE C	36"	24"	30"	3"	38"	26"

• MINIMUM
NOTE: STACKABLE BOXES ARE PERMITTED

BEFORE THE INSTALLATION OF THE #57 AGGREGATE AND JUNCTION BOX, THE CONTRACTOR SHALL INSTALL GEOTEXTILE FILTER FABRIC TYPE IV IN THE HOLE. THE FABRIC SHALL EXTEND TO JUST BELOW THE LIP OF THE JUNCTION BOX AND SHALL BE CONTINUOUSLY ADHERED TO THE EXTERIOR OF THE BOX WITH ADHESIVE. ANY LOCATIONS WHERE CONDUITS ENTER THE BOX, THE FABRIC SHALL BE 'X CUT' ONLY AS MUCH AS NECESSARY TO ALLOW PASSAGE OF EACH INDIVIDUAL CONDUIT THROUGH THE FABRIC. THE FABRIC SHALL BE INCIDENTAL TO BID ITEMS 481I, 2039INS835, OR 20392NS835.



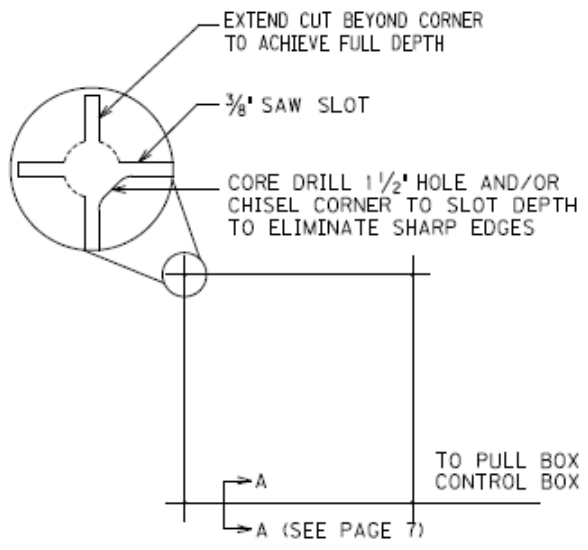
JUNCTION BOX INSTALLATION FOR
CONVENTIONAL LIGHTING OR TRAFFIC SIGNALS



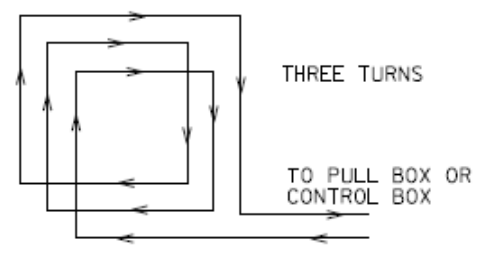
CONDUIT AND WARNING TAPE TRENCH

Traffic Signal Loop Detectors

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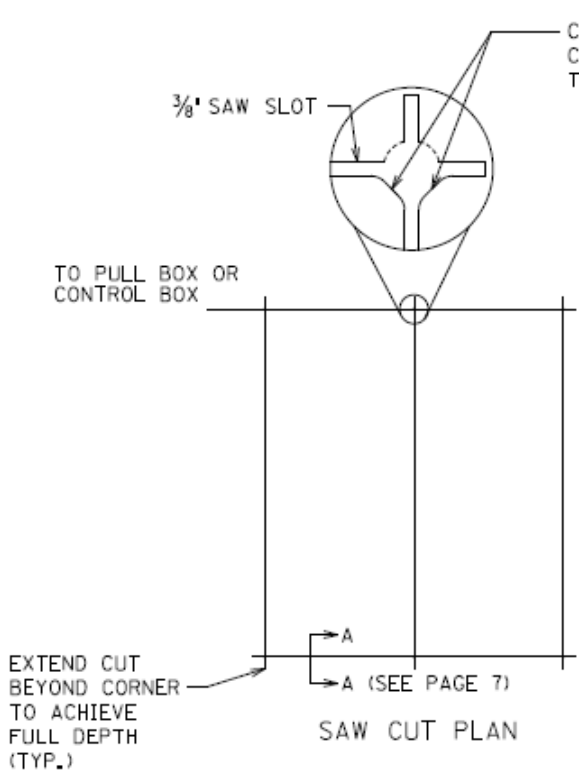


SAW CUT PLAN

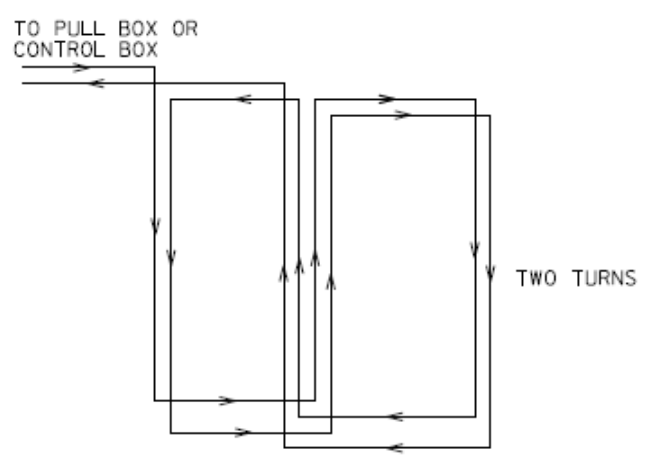


WIRING PLAN

6'X6' LOOP



SAW CUT PLAN

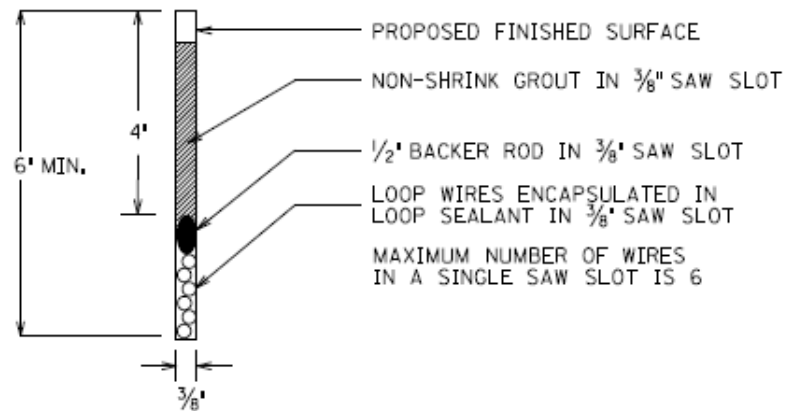


WIRING PLAN

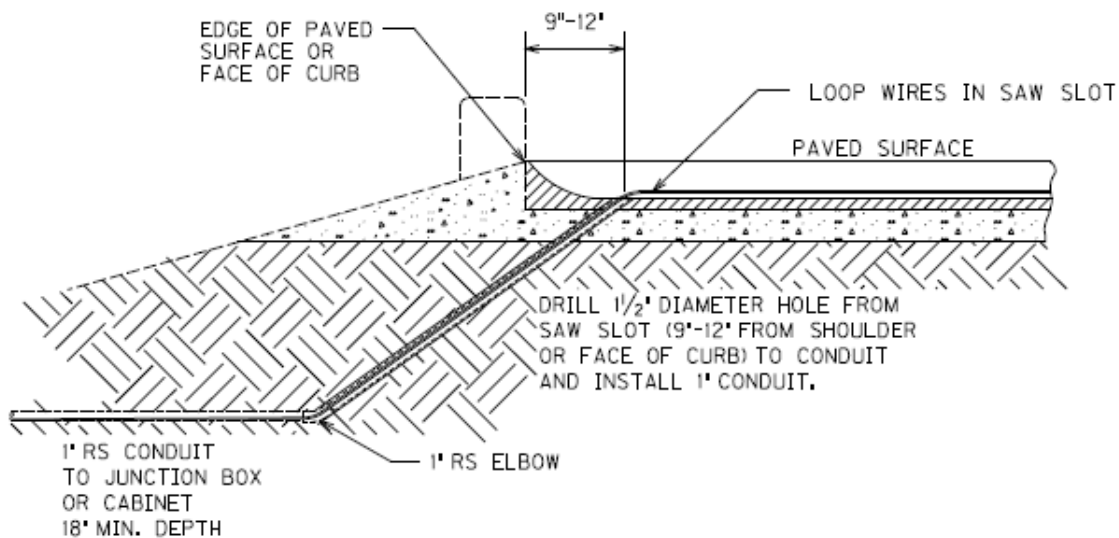
6'X30' QUADRAPOLE LOOP

Traffic Signal Loop Detectors

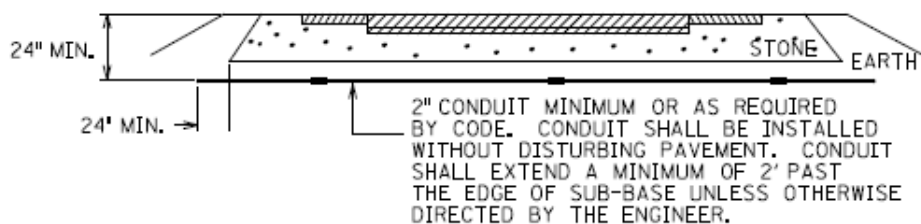
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SECTION A-A (SAW SLOT DETAIL)



SAW SLOT EDGE OF PAVEMENT TRANSITION



CONDUIT UNDER EXISTING PAVEMENT DETAIL

SPECIAL NOTES FOR TRAFFIC SIGNAL LOOP DETECTORS CITY OF LOUISVILLE

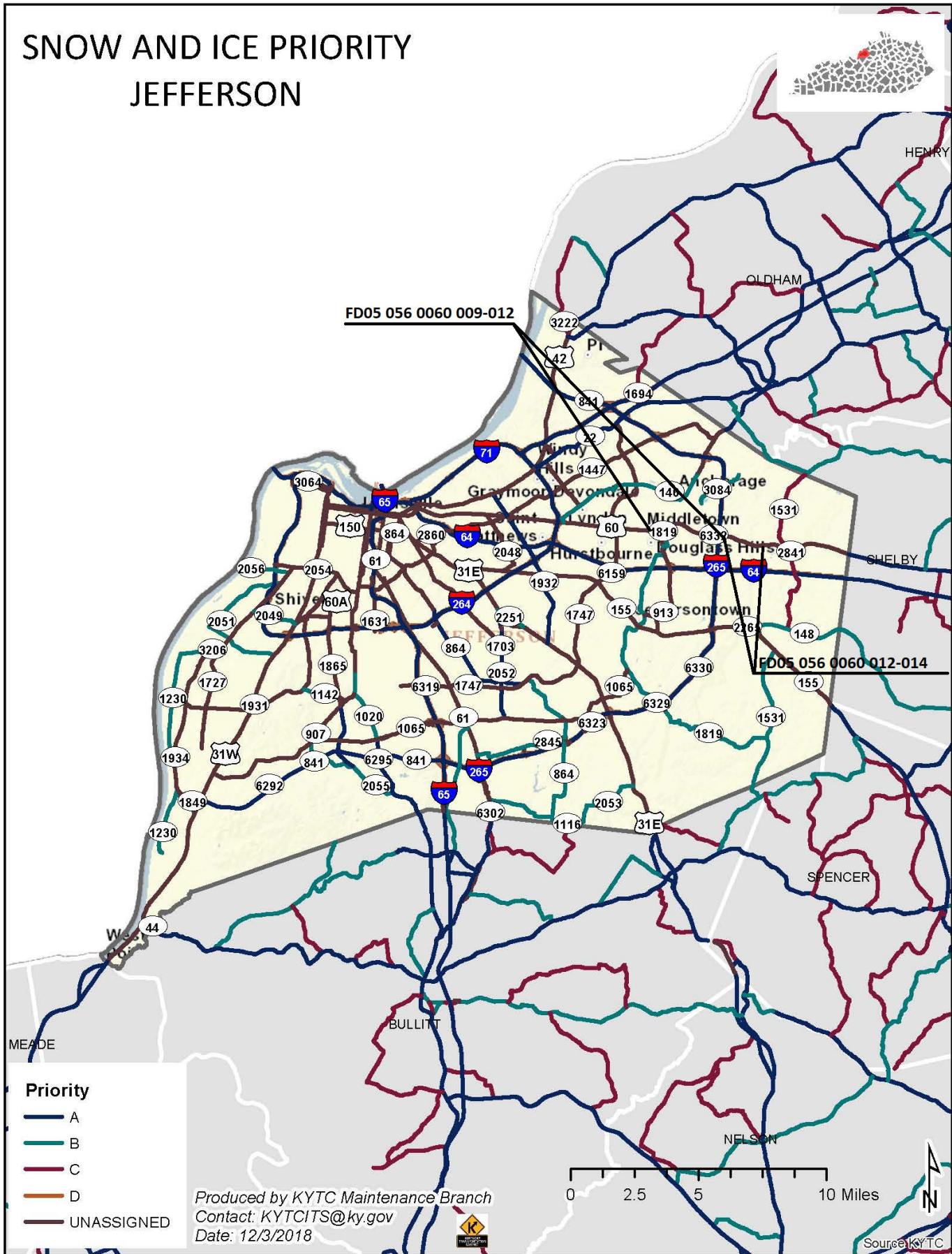
Be advised, existing traffic signal loop detectors are within the construction limits of this project. Notify the Engineer in writing, (2) weeks prior to beginning any work on the project. Install and test the new signal loops according to the Special Notes for Traffic Signal Loop Replacement.

The Engineer will contact and maintain liaison with the District Traffic Engineer and the City of Louisville to coordinate any necessary work.

On projects that include milling of roadways with existing traffic signal loops and if after milling the remnant contents of the existing saw slot (grout, loop wires, backer rod, and/or loop sealant) are not intact and flush with or below the top of the milled portion of the asphalt and with the saw slot completely filled with fines from the milling operation, clear the saw slot of loose remnant contents and refill the saw slot with natural sand. Obtain the Engineer's approval of the stabilized saw slot prior to resurfacing. The Department will not measure for separate payment clearing the saw slot and refilling with natural sand, but shall be incidental to Asphalt Pavement Milling and Texturing.

1-3893 Louisville Traffic Signal Loops
01/02/2012

SNOW AND ICE PRIORITY JEFFERSON



MATERIAL SUMMARY

CONTRACT ID: 192117

056GR19P025

MP05600601901

SHELBYVILLE ROAD (US 60) BEGIN APPROXIMATELY 250 FEET EAST OF KY 1819 EXTENDING EAST TO ENGLISH STATION ROAD ASPHALT RESURFACING, A DISTANCE OF 1.95 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0005	02562	TEMPORARY SIGNS	570.00	SQFT
0010	02650	MAINTAIN & CONTROL TRAFFIC - (9.722 - 11.674)	1.00	LS
0015	00342	CL4 ASPH SURF 0.38A PG76-22	7,196.00	TON
0020	06565	PAVE MARKING-THERMO X-WALK-6 IN	4,430.00	LF
0025	00190	LEVELING & WEDGING PG64-22	362.00	TON
0030	02676	MOBILIZATION FOR MILL & TEXT - (9.722 - 11.674)	1.00	LS
0035	02677	ASPHALT PAVE MILLING & TEXTURING	6,834.00	TON
0040	06568	PAVE MARKING-THERMO STOP BAR-24IN	1,096.00	LF
0045	03240	BASE FAILURE REPAIR	95.00	SQYD
0050	06514	PAVE STRIPING-PERM PAINT-4 IN	59,088.00	LF
0055	06510	PAVE STRIPING-TEMP PAINT-4 IN	118,176.00	LF
0060	06567	PAVE MARKING-THERMO STOP BAR-12IN	900.00	LF
0065	06574	PAVE MARKING-THERMO CURV ARROW	74.00	EACH
0070	06573	PAVE MARKING-THERMO STR ARROW	9.00	EACH
0075	20782NS714	PAVE MARKING THERMO-BIKE	8.00	EACH
0080	02775	ARROW PANEL	4.00	EACH
0085	04895	LOOP SAW SLOT AND FILL - (REVISED: 3-7-19)	5,070.00	LF
0090	02569	DEMOBILIZATION	1.00	LS
0095	24970EC	ASPHALT MATERIAL FOR TACK NON-TRACKING	30.00	TON
0100	04830	LOOP WIRE - (REVISED: 3-7-19)	12,595.00	LF
0105	04820	TRENCHING AND BACKFILLING - (REVISED: 3-7-19)	405.00	LF
0110	23625EC	PAVE MARK THERMO-6 IN W CAT TRAXX	75.00	LF
0115	06600	REMOVE PAVEMENT MARKER TYPE V	825.00	EACH
0120	10030NS	ASPHALT ADJUSTMENT	28,133.00	DOLL
0125	10020NS	FUEL ADJUSTMENT	11,201.00	DOLL
0130	04792	CONDUIT-1 IN - (ADDED: 3-7-19)	260.00	LF
0135	04811	ELECTRICAL JUNCTION BOX TYPE B - (ADDED: 3-7-19)	23.00	EACH
0140	04850	CABLE-NO. 14/1 PAIR - (ADDED: 3-7-19)	8,375.00	LF
0145	04960	REMOVE AND REPLACE SIDEWALK - (ADDED: 3-7-19)	8.00	SQYD
0150	24900EC	PVC CONDUIT-1 1/4 IN-SCHEDULE 80 - (ADDED: 3-7-19)	420.00	LF
0155	24963ED	LOOP TEST - (ADDED: 3-7-19)	41.00	EACH

MATERIAL SUMMARY

CONTRACT ID: 192117

056GR19P025

MP05600601902

SHELBYVILLE ROAD (US 60) BEGIN 200 FEET WEST OF THE INTERSECTION WITH ENGLISH STATION WAY/
BECKLEY WOODS DRIVE EXTENDING EAST TO 200' WEST OF FLOYDS FORK BRIDGE ASPHALT
RESURFACING, A DISTANCE OF 1.37 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0160	02562	TEMPORARY SIGNS	410.00	SQFT
0165	02650	MAINTAIN & CONTROL TRAFFIC - 12.253 - 13.632	1.00	LS
0170	00342	CL4 ASPH SURF 0.38A PG76-22	4,486.00	TON
0175	06600	REMOVE PAVEMENT MARKER TYPE V	350.00	EACH
0180	00190	LEVELING & WEDGING PG64-22	220.00	TON
0185	02676	MOBILIZATION FOR MILL & TEXT - (12.253 - 13.632)	1.00	LS
0190	02677	ASPHALT PAVE MILLING & TEXTURING	4,486.00	TON
0195	23625EC	PAVE MARK THERMO-6 IN W CAT TRAXX	63.00	LF
0200	06574	PAVE MARKING-THERMO CURV ARROW	40.00	EACH
0205	06568	PAVE MARKING-THERMO STOP BAR-24IN	280.00	LF
0210	06565	PAVE MARKING-THERMO X-WALK-6 IN	1,830.00	LF
0215	06510	PAVE STRIPING-TEMP PAINT-4 IN	81,882.00	LF
0220	06514	PAVE STRIPING-PERM PAINT-4 IN	40,941.00	LF
0225	03240	BASE FAILURE REPAIR	14.00	SQYD
0230	02775	ARROW PANEL	2.00	EACH
0235	04895	LOOP SAW SLOT AND FILL - (REVISED: 3-7-19)	2,470.00	LF
0240	04830	LOOP WIRE - (REVISED: 3-7-19)	6,085.00	LF
0245	02569	DEMOBILIZATION	1.00	LS
0250	04820	TRENCHING AND BACKFILLING - (REVISED: 3-7-19)	235.00	LF
0255	02696	SHOULDER RUMBLE STRIPS	14,562.00	LF
0260	10030NS	ASPHALT ADJUSTMENT	18,399.00	DOLL
0265	10020NS	FUEL ADJUSTMENT	7,325.00	DOLL
0270	24970EC	ASPHALT MATERIAL FOR TACK NON-TRACKING	19.00	TON
0275	04792	CONDUIT-1 IN - (ADDED: 3-7-19)	135.00	LF
0280	04811	ELECTRICAL JUNCTION BOX TYPE B - (ADDED: 3-7-19)	12.00	EACH
0285	04850	CABLE-NO. 14/1 PAIR - (ADDED: 3-7-19)	3,405.00	LF
0290	04960	REMOVE AND REPLACE SIDEWALK - (ADDED: 3-7-19)	5.00	SQYD
0295	24900EC	PVC CONDUIT-1 1/4 IN-SCHEDULE 80 - (ADDED: 3-7-19)	235.00	LF
0300	24963ED	LOOP TEST - (ADDED: 3-7-19)	20.00	EACH

FD05 056 0060 009-012

THERMOPLASTIC INTERSECTION PAVEMENT MARKINGS SUMMARY

MPT.	INTERSECTION	X-WALKS		STOP BARS		CURVE		ARROWS		COMB		CAT		BIKE		RAILROAD	NOTES
		6 INCH	24 INCH	12 INCH	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA		
9.74	CENTER TURN						2										
9.773	CENTER TURN						2										
9.803	SHOULDER EB							1							1		
9.805	1 CENTER, 1 WB SHOULDER						2										
9.807	SHOULDER WB							1							1		
9.812	CENTER TURN						2										
9.835	SHOULDER WB							1							1		
9.857	SHOULDER EB							1							1		
9.874	CENTER TURN						1										
9.876	SHOULDER EB						1										
9.91	MADISON AVE	660	120				2								1		
9.92	1 CENTER, 1 WB SHOULDER						2										
9.926	SHOULDER EB							1									
9.940	CENTER TURN																
9.945	SHOULDER WB							1							1		
9.979	SHOULDER EB							1							1		
10.002	SHOULDER EB						1								1		
10.019	SHOULDER EB AND WB						1		2						1		
10.04	1 CENTER, 1 WB SHOULDER						2										
10.046	CENTER TURN						1										
10.077	CENTER TURN						2										
10.1	CENTER TURN						2										
10.135	CENTER TURN						2										
10.175	CENTER TURN						2										
10.215	CENTER TURN						2										
10.257	CENTER TURN						2										
10.322	CENTER TURN						2										
10.366	EVERGREEN ROAD						1										
10.384	EVERGREEN ROAD						1										
10.389	EVERGREEN ROAD	480	80	900													
10.425	EVERGREEN ROAD						1										
10.482	CENTER TURN						2										
10.679	JUNEAU DRIVE						1										
10.685	JUNEAU DRIVE						2										
10.702	JUNEAU DRIVE						2										
10.733	JUNEAU DRIVE	670	488														
10.75	LEFT TURN						1										
10.768	LEFT TURN						1										
10.765	RIGHT TURN						1										
10.832	CENTER TURN						2										
10.893	SHOPPING CENTER						1										
10.908	SHOPPING CENTER						1										
10.92	SHOPPING CENTER	340	84														
10.935	SHOPPING CENTER						1										
10.945	SHOPPING CENTER						1										
11.043	CENTER TURN						2										
11.171	TOWNE CREEK ROAD						1										
11.185	TOWNE CREEK ROAD						1										
11.2	TOWNE CREEK ROAD	600	108														
11.215	TOWNE CREEK ROAD						2										
11.223	LEFT TURN						1										
11.228	RIGHT TURN						1										
11.368	MERIDIAN/SHOPPING CTR						2										
11.381	MERIDIAN/SHOPPING CTR						2										
11.385	MERIDIAN/SHOPPING CTR						1										
11.401	MERIDIAN/SHOPPING CTR	840	108														
11.422	MERIDIAN/SHOPPING CTR						2										
11.44	MERIDIAN/SHOPPING CTR						2										
11.644	ENGLISH ST./URTON LN.						2										
11.66	ENGLISH ST./URTON LN.	840	108				2										
11.684	ENGLISH ST./URTON LN.	4430	1096	900			74	9							75	75	8

Traffic Loops

*Insert info in highlighted areas

Traffic Loop

# Loops	MP	Description	Saw Lead Wire	Dig Lead Wire
5	9.91	MADISON AVE	600	50
8	10.389	EVERGREEN ROAD	400	50
4	10.733	JUNEAU DRIVE	200	50
5	11.2	TOWNE CREEK ROAD	500	50
6	11.401	MERIDIAN/SHOPPING CENTER	600	50
7	11.684	ENGLISH STATION/URTON AVE.	700	50

PLAN QUANTITIES

SAW, SLOT AND FILL	LOOP WIRE	CONDUIT	TRENCHING
LF	LF	LF	LF
1110	2620	50	50
1216	3012	50	50
608	1556	50	50
1010	2420	50	50
1212	2884	50	50
1414	3348	50	50

TOTAL 6570 15840 300 300

FD05 056 0060 009-012

Water, Gas & Manhole Locations

Eastbound Lane			Westbound Lane		
Type	MP	Cross Street (if available)	Type	MP	Cross Street (if available)
MH	9.730	SHOULDER	CB	9.754	CURB
CB	9.739	CURB	CB	9.793	OLD HARRODS CR RD
CB	9.757	CURB	MH(3)	9.793	OLD HARRODS CR RD
CB	9.800	CENTER	CB	9.806	CURB
CB	9.910	MADISON	CB	9.82	CURB
WV	9.910	MADISON	CB	9.88	CURB
CB(2)	9.958	CURB,CENTER	CB	10.036	BLISS AVE
MH	9.968	SHOULDER	GV	10.389	EVERGREEN
CB	10.010	CURB	MH	10.389	EVERGREEN
CB	10.017	CENTER	CB	10.492	COMMERCIAL ENT
CB	10.024	CURB	MH	10.512	SHOULDER
MH	10.024	SHOULDER	MH	10.572	SHOULDER
WV(3)	10.030	SHOPPING CTR	WV	10.6	PROFESSIONAL PARK
GV	10.030	SHOPPING CTR	MH	10.6	PROFESSIONAL PARK
WV	10.046	HYDRANT	MH	10.917	KROGER
CB	10.068	CURB	WV	11.08	KROGER
MH	10.075	SHOPPING CTR	MH	11.542	WAL MART
CB	10.106	CURB			
CB	10.160	CURB			
CB	10.190	CURB			
CB(2)	10.210	CURB, CENTER			
MH	10.210	SHOULDER			
CB	10.225	KRATZ AVE			
MH	10.225	KRATZ AVE			
CB	10.254	CURB			
CB	10.266	CENTER			
CB	10.276	CURB			
CB	10.294	CURB			
CB	10.328	CURB			
CB	10.354	CENTER			
CB	10.364	ENTRANCE			
CB	10.389	EVERGREEN			
MH	10.477	SHOULDER			
CB	10.528	SHOULDER			
MH	10.552	SHOULDER			
GV	10.675	URTON LANE			
CB	10.740	CURB			
CB	10.804	CURB			

FD05 056 0060 012-014

Traffic Loops

*Insert info in highlighted areas

Traffic Loop

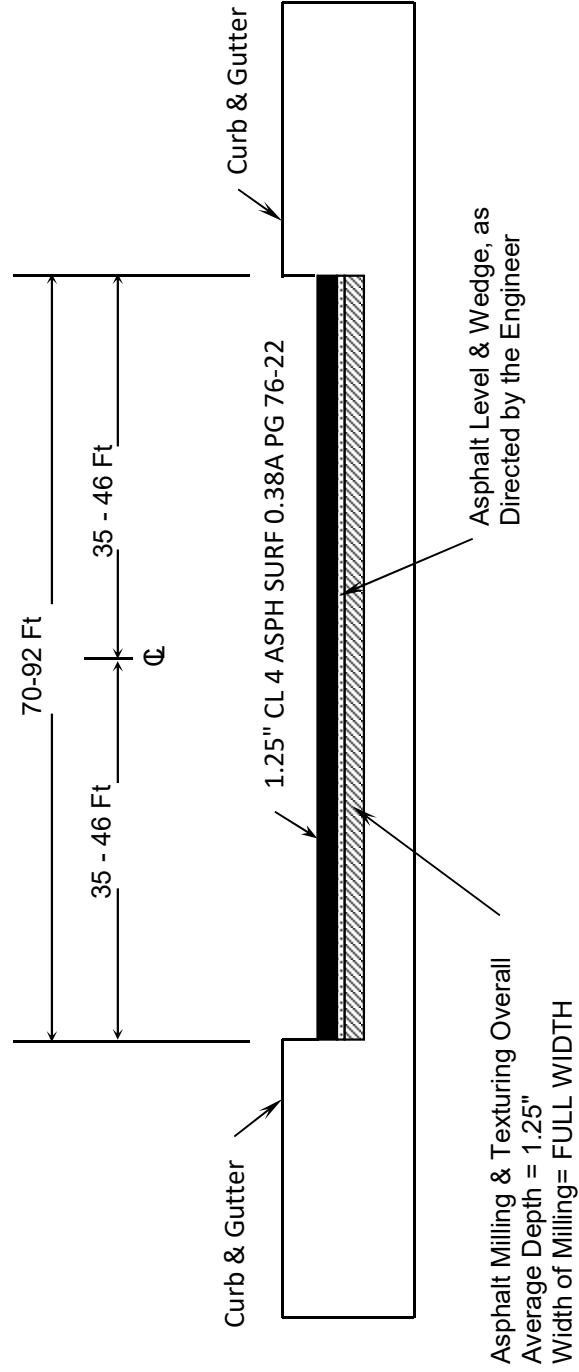
# Loops	MP	Description	Saw Lead Wire	Dig Lead Wire
2	12.291	BECKLEY WOODS	200	50
2	12.58	SOUTH ENGLISH STATION	200	50
2	12.895	BECKLEY STATION	200	50

PLAN QUANTITIES

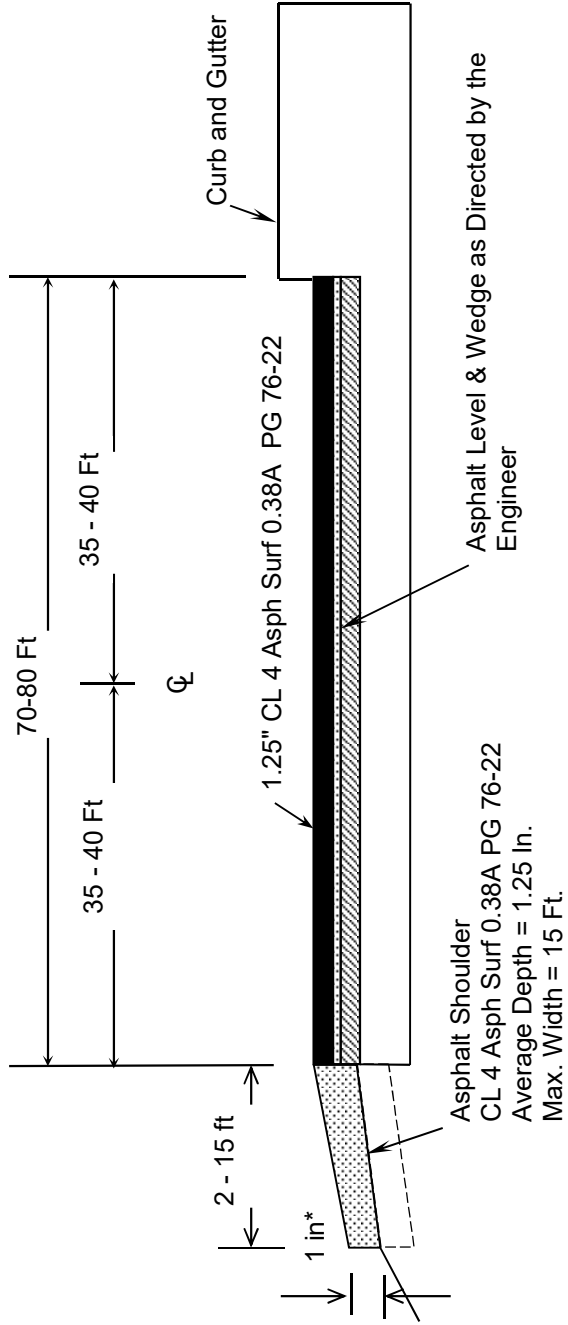
SAW, SLOT AND FILL	LOOP WIRE	CONDUIT	TRENCHING
LF	LF	1 1/4 INCH LF	LF
404	1028	50	50
404	1028	50	50
404	1028	50	50

TOTAL 1212 3084 150 150

Jefferson County
TYPICAL SECTION
FD05 056 0060 009-012
MP's 9.722 - 10.988

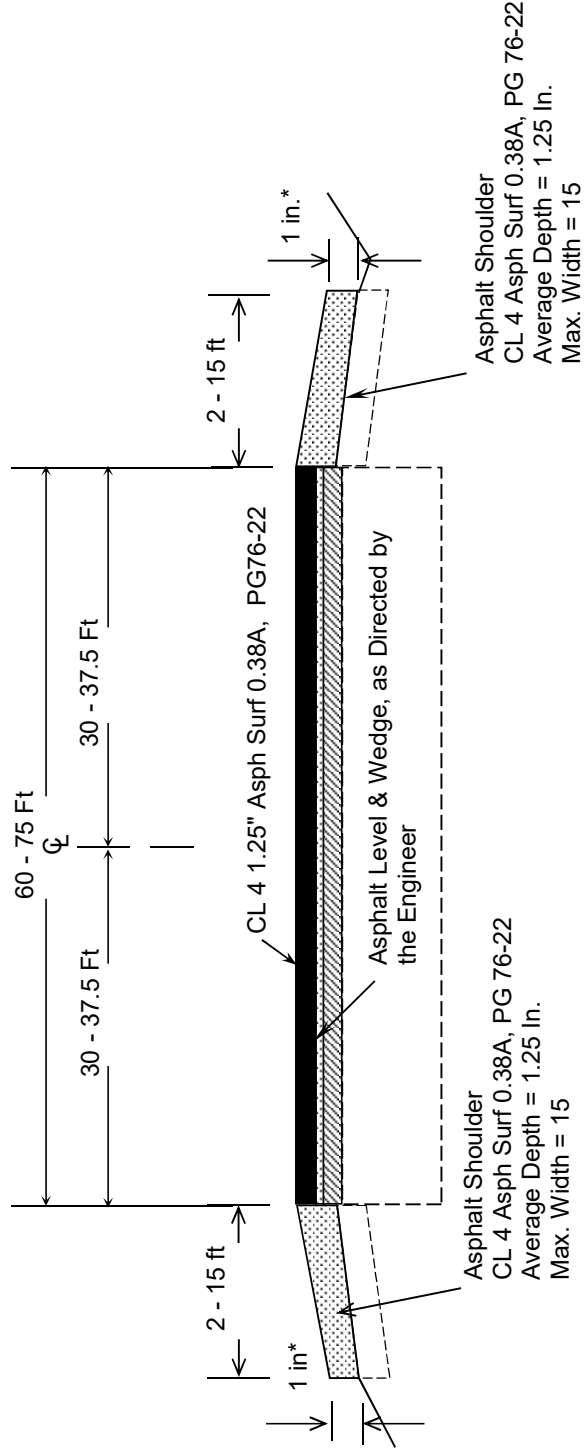


Jefferson County
TYPICAL SECTION
FD05 056 0060 009-012
MP's 10.988 - 11.030
MP's 11.205 - 11.240
MP's 11.381 - 11.470
MP's 11.548 - 11.858
MP's 11.636 - 11.674



Asphalt Milling & Texturing Overall
 Average Depth = 1.25 INCHES
 Width of Milling= FULL WIDTH

Jefferson County
TYPICAL SECTION
FD05 056 0060 009-012
MP's 11.030 - 11.205
MP's 11.240 - 11.381
MP's 11.470 - 11.548
MP's 11.585 - 11.636

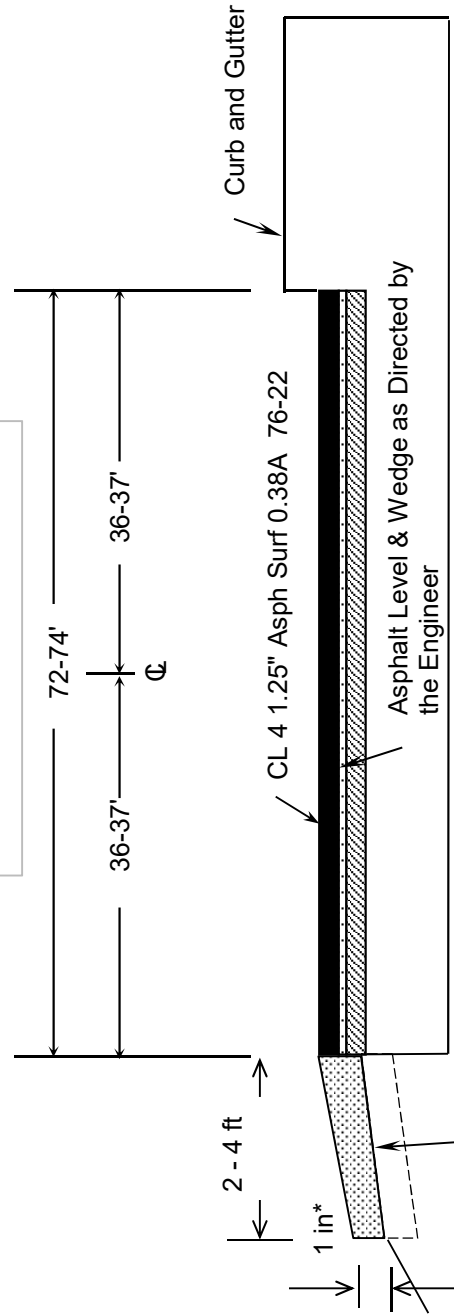


Asphalt Milling & Texturing Overall
 Average Depth = 1.25 INCHES
 Width of Milling= FULL WIDTH

Asphalt Shoulder
 CL 4 Asph Surf 0.38A, PG 76-22
 Average Depth = 1.25 in.
 Max. Width = 15

Asphalt Shoulder
 CL 4 Asph Surf 0.38A, PG 76-22
 Average Depth = 1.25 in.
 Max. Width = 15

Jefferson County
TYPICAL SECTION
FD05 056 0060 012-014
MP's 12.253 - 12.313
MP's 12.534 - 12.580
MP's 12.920 - 12.996

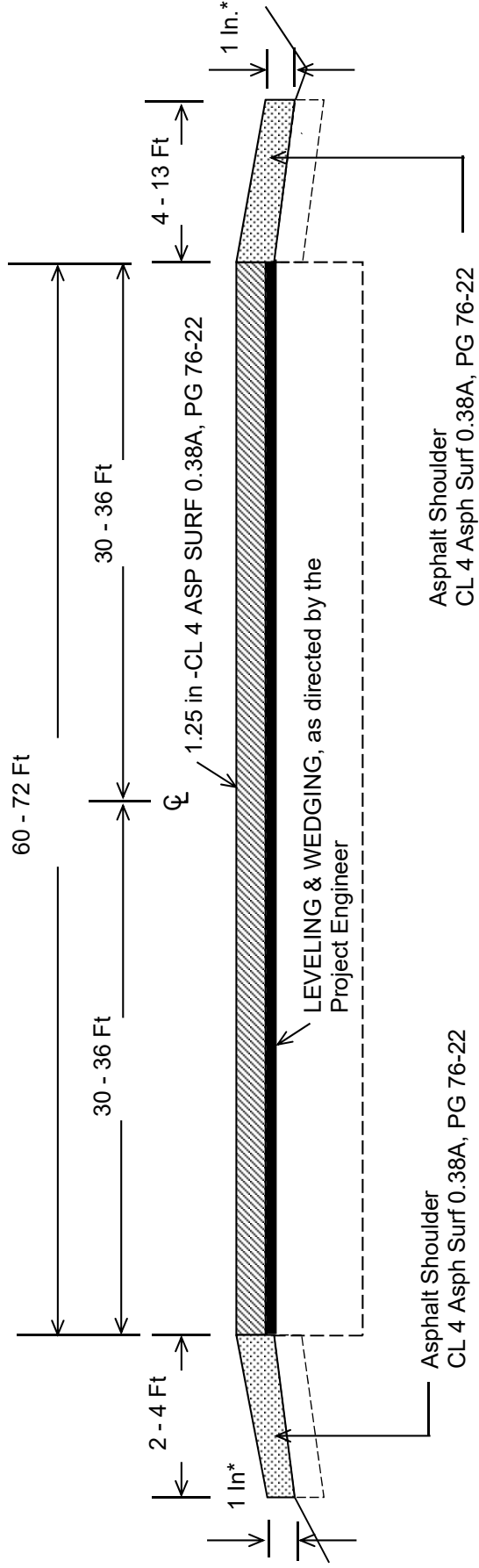


Asphalt Shoulder
 CL 4 Asphalt Surf 0.38A PG 76-22
 Average Depth = 1.25 In.
 Max. Width = 4 Ft.

Asphalt Milling & Texturing Overall
 Average Depth = 1.25 INCHES
 Width of Milling = FULL WIDTH

Asphalt Level & Wedge as Directed by
 the Engineer

Jefferson County
TYPICAL SECTION
FD05 056 0060 012-014
MP's 12.313 - 12.534
MP's 12.580 - 12.920
MP's 12.996 - 13.632



Average Depth = 1.25 In.
Max. Width = 4 Ft.

Average Depth = 1.25 In.
Max. Width = 13 Ft.

***Where Existing Site Conditions Permit**

PART II
SPECIFICATIONS AND STANDARD DRAWINGS

SPECIFICATIONS REFERENCE

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2012* and *Standard Drawings, Edition of 2016*.

SUPPLEMENTAL SPECIFICATIONS

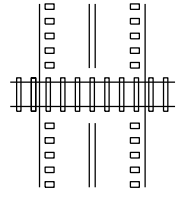
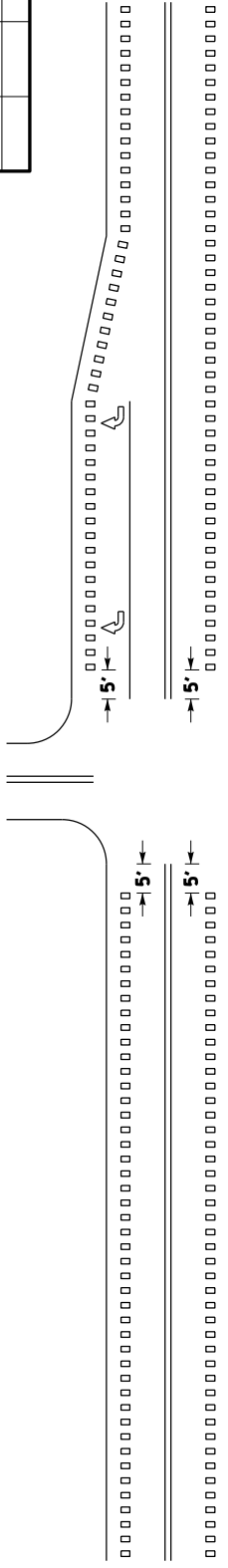
The contractor shall use the Supplemental Specifications that are effective at the time of letting.
The Supplemental Specifications can be found at the following link:

<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

2016 KENTUCKY STANDARD DRAWINGS

CURVE WIDENING AND SUPERELEVATION TRANSITIONS	RGS-001-07
SUPERELEVATION FOR MULTILANE PAVEMENT	RGS-002-06
MISCELLANEOUS STANDARDS	RGX-001-06
APPROACHES, ENTRANCES, AND MAIL BOX TURNOUT	RPM-110-07
SHOULDER AND EDGE LINE RUMBLE STRIP DETAILS	TPM-165
LANE CLOSURE TWO-LANE HIGHWAY	TTC-100-04
SHOULDER CLOSURE	TTC-135-02
PAVEMENT CONDITION WARNING SIGNS	TTD-125-02
MOBILE OPERATION FOR PAINT STRIPING CASE I	TTS-100-02
MOBILE OPERATION FOR PAINT STRIPING CASE II	TTS-105-02

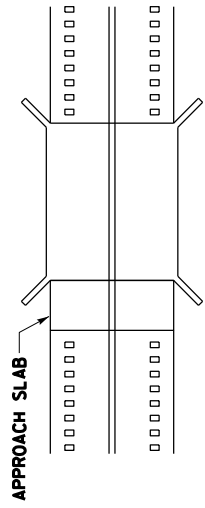
COUNTY OF	TOWNSHIP	SHEET NO.



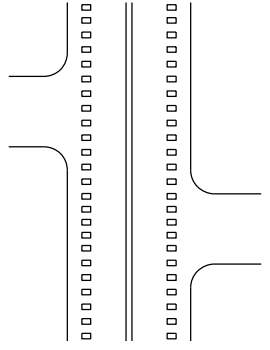
HIGHWAY-RAIL GRADE CROSSINGS ③

INTERSECTIONS WITH OR WITHOUT RIGHT TURN LANES ①

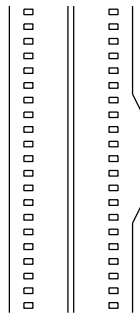
MARKED CROSSWALK ②



BRIDGE DECK/APPROACH SLAB ④



DRIVEWAYS/MINOR COMMERCIAL ENTRANCES ⑤



MAILBOX TURNOUTS ⑥

~ NOTES ~

- ① RUMBLE STRIPS SHALL BE OMITTED THROUGH MAJOR INTERSECTIONS WITH, OR WITHOUT, RIGHT-TURN LANES. OMIT RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF THE AREA WHERE EDGELINE PAVEMENT MARKINGS HAVE BEEN OMITTED (NORMALLY WHERE SIDE STREET RADIUS INTERSECTS MAINLINE).
- ② RUMBLE STRIPS SHALL NOT BE INSTALLED THROUGH MARKED CROSSWALKS. OMIT RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF MARKED CROSSWALKS.
- ③ RUMBLE STRIPS SHALL NOT BE INSTALLED ACROSS HIGHWAY-RAIL GRADE CROSSINGS.
- ④ RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS OR APPROACH SLABS.
- ⑤ RUMBLE STRIPS SHALL BE INSTALLED THROUGH DRIVEWAYS & MINOR COMMERCIAL ENTRANCES.
- ⑥ RUMBLE STRIPS SHALL BE INSTALLED THROUGH MAILBOX TURNOUTS.
7. RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

DRAWING NOT TO SCALE

USE WITH SEP1A 006, 007, AND 008

KENTUCKY
DEPARTMENT OF HIGHWAYS
SHOULDER & EDGELINE
RUMBLE STRIP DETAILS

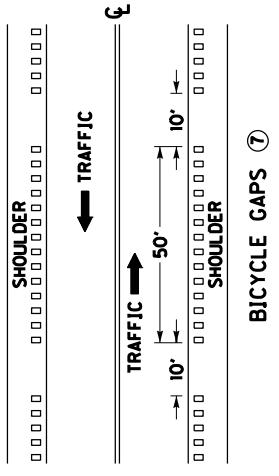
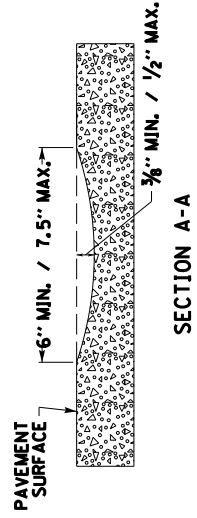
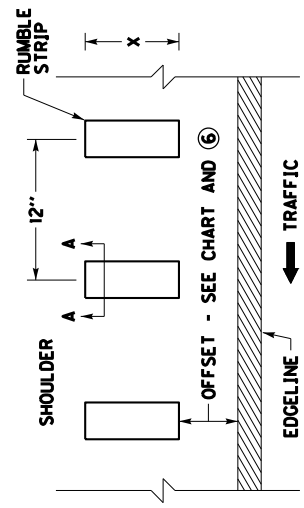
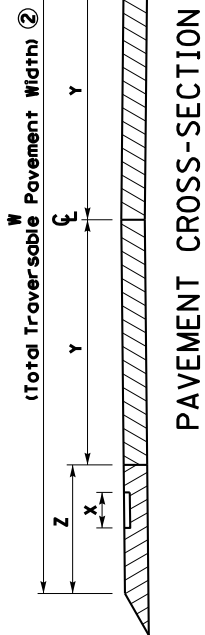
BID ITEMS AND UNIT TO BID
EDGELINE RUMBLE STRIPS
SHOULDER RUMBLE STRIPS

LF
LF

SUBMITTED: *B. [Signature]*
DATE: 11-23-16
005

COUNTY OF	TIERING	SHEET NO.
-----------	---------	-----------

PAVEMENT WIDTH (W) ②	RUMBLE LENGTH (X) ⑤	OFFSET ⑥	CLRS & SRS	
			LANE WIDTH (Y) ③	SHOULDER WIDTH (Z) ④
34'	8"	6"	12'	5'
35'	8"	6"	12'	5.5'
36'	8"	6"	12'	6'
37'	12"	12"	12'	6.5'
38'	12"	12"	12'	7'
39'	12"	12"	12'	7.5'
>=40'	16"	12"	12'	>=8'



NOTES

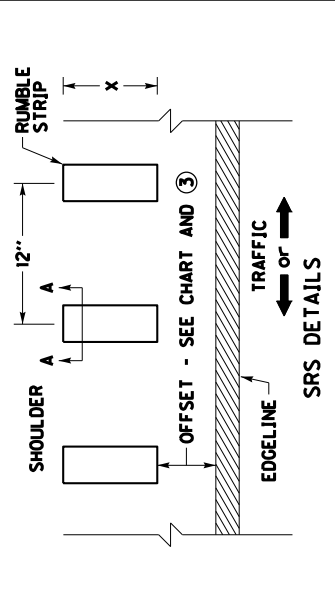
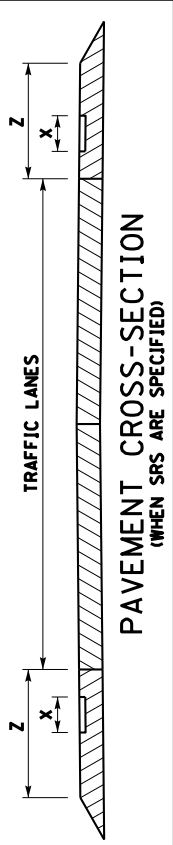
- SHOULDER RUMBLE STRIPS SHOULD BE INSTALLED ACCORDING TO THE DIMENSIONS PROPOSED ABOVE UNLESS THERE IS AN ENGINEERING BASIS THAT SUPPORTS A CHANGE IN DIMENSION. FOR EXAMPLE, IF THE EXISTING LANE WIDTH IS NARROWER THAN THE LANE WIDTH PROPOSED IN THIS DRAWING AND THE EXISTING SHOULDER PAVEMENT DEPTH IS NOT SUITABLE TO BE CONVERTED INTO A PORTION OF THE PROPOSED LANE WIDTH, THEN THE EXISTING LANE WIDTH SHOULD BE USED INSTEAD OF THE WIDTH PROPOSED IN THIS DRAWING.
- PAVEMENT WIDTH (W) IS THE TOTAL WIDTH OF TRAVERSABLE PAVEMENT. DO NOT INCLUDE THE WIDTH OF ANY NON-TRAVERSABLE PAVEMENT, SUCH AS PAVEMENT WEDGES, WHEN MEASURING THE PAVEMENT WIDTH (W).
- LANE WIDTH (Y) TO BE MEASURED FROM CENTER OF ROAD TO CENTER OF EDGE LINE STRIPE.
- PAVED SHOULDER WIDTH (Z) TO BE MEASURED FROM CENTER OF EDGE LINE STRIPE TO OUTSIDE EDGE OF TRAVERSABLE PAVEMENT. DISTANCES SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE. IF THE TYPICAL SECTION SHOWS A LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) THAT DIFFERS FROM THE WIDTHS LISTED IN THIS DRAWING, THE ENGINEER SHALL DETERMINE THE LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) AT THE TIME OF CONSTRUCTION.
- NOTE: CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED WHEN THE LANE WIDTH (Y) IS LESS THAN 11 FT.
- RUMBLE LENGTH (X) AND/OR OFFSET DISTANCE MAY BE MODIFIED AS THE ENGINEER DIRECTS, IF THE SHOULDER WIDTH (Z) IS EQUAL TO OR LESS THAN THE COMBINED WIDTH OF THE PROPOSED RUMBLE LENGTH (X) AND OFFSET DISTANCE.
- ALL SHOULDER RUMBLE STRIPS ALONG SHOULDERS THAT ARE 5' OR WIDER SHOULD INCLUDE BICYCLE GAPS AS DETAILED.
- SHOULDER RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

DRAWING NOT TO SCALE

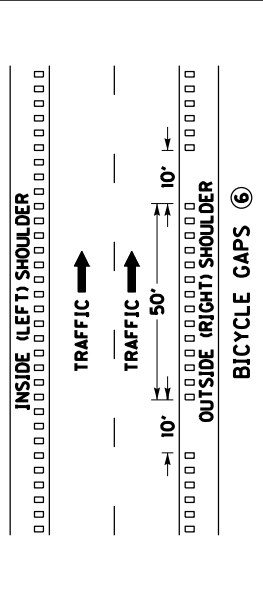
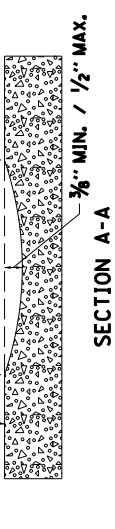
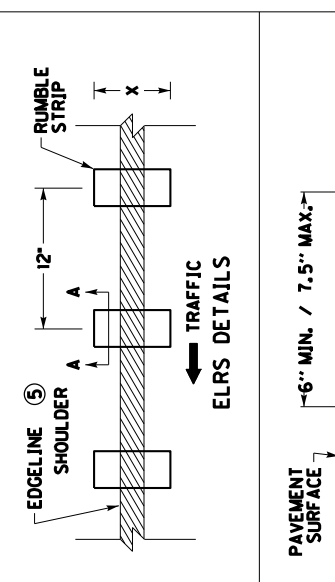
USE WITH SEP1A 005
KENTUCKY
DEPARTMENT OF HIGHWAYS
**SHOULDER RUMBLE STRIP
DETAILS
TWO LANE ROADWAYS**
SUBMITTED: *B. [Signature]* DATE: 11-23-16
007

BID ITEM AND UNIT TO BID
SHOULDER RUMBLE STRIPS
LF

COUNTY OF	TITLING	SHEET NO.
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SHOULDER WIDTH (Z) ②	RUMBLE TYPE ①	RUMBLE LENGTH (X) ③	OFFSET ④
>1'	ELRS	8"	N/A
2'	ELRS-N/A SRS-6"	8"	ELRS-N/A SRS-6"
3'	ELRS or SRS	8"	ELRS-N/A SRS-6"
4'	ELRS or SRS	8"	ELRS-N/A SRS-6"
5'	SRS ⑥	8"	6"
6'	SRS ⑥	8"	6"
7'	SRS ⑥	12"	12"
>8'	SRS ⑥	16"	12"



- NOTES ~
- FOR MULTI-LANE ROADWAYS, THE RUMBLE TYPE TO BE INSTALLED IS BASED ON SHOULDER WIDTH (Z), FOR SHOULDER WIDTHS OF 2', 3', AND 4' THE RUMBLE TYPE MAY BE SPECIFIED AS EITHER EDGE LINE RUMBLE STRIPS (ELRS) OR SHOULDER RUMBLE STRIPS (SRS). IN THESE SITUATIONS, THE RUMBLE TYPE TO BE INSTALLED WILL BE SPECIFIED IN THE PLANS, PROPOSAL, AND/OR BID ITEMS, OR AS DIRECTED BY THE ENGINEER.
 - WHEN ELRS ARE SPECIFIED, SHOULDER WIDTH (Z) IS FROM LANE SIDE EDGE OF RUMBLE STRIP TO OUTSIDE EDGE OF TRAVERSABLE PAVEMENT.
 - WHEN SRS ARE SPECIFIED, SHOULDER WIDTH (Z) IS FROM CENTER OF EDGE LINE STRIPE TO OUTSIDE EDGE OF TRAVERSABLE PAVEMENT.
 - RUMBLE LENGTH (X) AND/OR OFFSET DISTANCE MAY BE MODIFIED AS THE ENGINEER DIRECTS. IF THE SHOULDER WIDTH (Z) IS EQUAL TO OR LESS THAN THE COMBINED WIDTH OF THE PROPOSED RUMBLE LENGTH (X) AND OFFSET DISTANCE.
 - DISTANCES SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE.
 - WHEN ELRS ARE SPECIFIED, THE EDGE LINE MARKING SHALL BE PLACED IN THE CENTER OF THE RUMBLE STRIP.
 - SHOULDER RUMBLE STRIPS (SRS) ALONG OUTSIDE (RIGHT) SHOULDERS THAT ARE 5' OR WIDER SHOULD INCLUDE BICYCLE GAPS AS DETAILED. BICYCLE GAPS ARE NOT REQUIRED ON INSIDE (LEFT) SHOULDERS. BICYCLE GAPS SHALL NOT BE USED ON INTERSTATES AND PARKWAYS.
 - RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS, OR WHEN THE SHOULDER WIDTH IS LESS THAN 1 FT.

BID ITEMS AND UNIT TO BID
SHOULDER RUMBLE STRIPS
EDGE LINE RUMBLE STRIPS

LF
LF

USE WITH SEP1A 005

KENTUCKY
DEPARTMENT OF HIGHWAYS

RUMBLE STRIP DETAILS
MULTI-LANE ROADWAYS
AND RAMP

SUBMITTED: *B. [Signature]* JL-23-16 DATE

008

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

**TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administrating agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

II. NONDISCRIMINATION OF EMPLOYEES

**AN ACT OF THE KENTUCKY
GENERAL ASSEMBLY TO PREVENT
DISCRIMINATION IN EMPLOYMENT
KRS CHAPTER 344
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

No present or former public servant shall, within six (6) months following termination of his office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 3 Fountain Place, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: January 27, 2017

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

FEDERAL MINIMUM WAGE

\$7.25 PER HOUR

BEGINNING JULY 24, 2009

OVERTIME PAY At least 1½ times your regular rate of pay for all hours worked over 40 in a workweek.

CHILD LABOR An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

No more than

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

TIP CREDIT Employers of “tipped employees” must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee’s tips combined with the employer’s cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

ENFORCEMENT The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act’s child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



1-866-4-USWAGE

(1-866-487-9243) TTY: 1-877-889-5627



WWW.WAGEHOUR.DOL.GOV

PART IV
INSURANCE

INSURANCE

The Contractor shall procure and maintain the following insurance in addition to the insurance required by law:

- 1) Commercial General Liability-Occurrence form – not less than \$2,000,000 General aggregate, \$2,000,000 Products & Completed Aggregate, \$1,000,000 Personal & Advertising, \$1,000,000 each occurrence.
- 2) Automobile Liability- \$1,000,000 per accident
- 3) Employers Liability:
 - a) \$100,000 Each Accident Bodily Injury
 - b) \$500,000 Policy limit Bodily Injury by Disease
 - c) \$100,000 Each Employee Bodily Injury by Disease
- 4) The insurance required above must be evidenced by a Certificate of Insurance and this Certificate of Insurance must contain one of the following statements:
 - a) "policy contains no deductible clauses."
 - b) "policy contains _____ (amount) deductible property damage clause but company will pay claim and collect the deductible from the insured."
- 5) **KENTUCKY WORKMEN'S COMPENSATION INSURANCE.** The contractor shall furnish evidence of coverage of all his employees or give evidence of self-insurance by submitting a copy of a certificate issued by the Workmen's Compensation Board.

The cost of insurance is incidental to all contract items. All subcontractors must meet the same minimum insurance requirements.

PART V
BID ITEMS

PROPOSAL BID ITEMS

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Report Date 3/7/19

Section: 0001 - PAVING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00190		LEVELING & WEDGING PG64-22	582.00	TON		\$	
0020	00342		CL4 ASPH SURF 0.38A PG76-22	11,682.00	TON		\$	
0030	02562		TEMPORARY SIGNS	980.00	SQFT		\$	
0040	02650		MAINTAIN & CONTROL TRAFFIC 12.253 - 13.632	1.00	LS		\$	
0050	02650		MAINTAIN & CONTROL TRAFFIC (9.722 - 11.674)	1.00	LS		\$	
0060	02676		MOBILIZATION FOR MILL & TEXT (12.253 - 13.632)	1.00	LS		\$	
0070	02676		MOBILIZATION FOR MILL & TEXT (9.722 - 11.674)	1.00	LS		\$	
0080	02677		ASPHALT PAVE MILLING & TEXTURING	11,320.00	TON		\$	
0090	02696		SHOULDER RUMBLE STRIPS	14,562.00	LF		\$	
0100	02775		ARROW PANEL	6.00	EACH		\$	
0110	03240		BASE FAILURE REPAIR	109.00	SQYD		\$	
0121	04792		CONDUIT-1 IN (ADDED: 3-7-19)	395.00	LF		\$	
0123	04811		ELECTRICAL JUNCTION BOX TYPE B (ADDED: 3-7-19)	35.00	EACH		\$	
0130	04820		TRENCHING AND BACKFILLING (REVISED: 3-7-19)	640.00	LF		\$	
0140	04830		LOOP WIRE (REVISED: 3-7-19)	18,680.00	LF		\$	
0145	04850		CABLE-NO. 14/1 PAIR (ADDED: 3-7-19)	11,780.00	LF		\$	
0150	04895		LOOP SAW SLOT AND FILL (REVISED: 3-7-19)	7,540.00	LF		\$	
0155	04960		REMOVE AND REPLACE SIDEWALK (ADDED: 3-7-19)	13.00	SQYD		\$	
0160	06510		PAVE STRIPING-TEMP PAINT-4 IN	200,058.00	LF		\$	
0170	06514		PAVE STRIPING-PERM PAINT-4 IN	100,029.00	LF		\$	
0180	06565		PAVE MARKING-THERMO X-WALK-6 IN	6,260.00	LF		\$	
0190	06567		PAVE MARKING-THERMO STOP BAR-12IN	900.00	LF		\$	
0200	06568		PAVE MARKING-THERMO STOP BAR-24IN	1,376.00	LF		\$	
0210	06573		PAVE MARKING-THERMO STR ARROW	9.00	EACH		\$	
0220	06574		PAVE MARKING-THERMO CURV ARROW	114.00	EACH		\$	
0230	06600		REMOVE PAVEMENT MARKER TYPE V	1,175.00	EACH		\$	
0240	10020NS		FUEL ADJUSTMENT	18,526.00	DOLL	\$1.00	\$	\$18,526.00
0250	10030NS		ASPHALT ADJUSTMENT	46,532.00	DOLL	\$1.00	\$	\$46,532.00
0260	20782NS714		PAVE MARKING THERMO-BIKE	8.00	EACH		\$	
0270	23625EC		PAVE MARK THERMO-6 IN W CAT TRAXX	138.00	LF		\$	
0272	24900EC		PVC CONDUIT-1 1/4 IN-SCHEDULE 80 (ADDED: 3-7-19)	655.00	LF		\$	
0274	24963ED		LOOP TEST (ADDED: 3-7-19)	61.00	EACH		\$	
0280	24970EC		ASPHALT MATERIAL FOR TACK NON- TRACKING	49.00	TON		\$	

Section: 0002 - DEMOBILIZATION

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PROPOSAL BID ITEMS

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LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0290	02569		DEMOBILIZATION	1.00	LS		\$	